



## Update Report of the Chief Planning Officer

### CITY PLANS PANEL

Date: 22 October 2020

**Subject: Planning Application 20/02048/FU for the demolition of existing structures, the construction of a new cafe and a cycle hub; a new taxi rank; public realm enhancement works including hard and soft landscaping; improvements to the cycle, pedestrian and multi-modal interchange arrangements; and the installation of new access lift at Bishopgate Street, New Station Street and Mill Goit, Neville Street, and Dark Neville Street, Leeds City Centre.**

And

**Listed Building Application 20/01996/LI for the demolition of an existing section of wall and the construction of a replacement wall, which will be built into an existing listed wall at Bishopgate Street, New Station Street, Leeds.**

#### APPLICANT

Leeds City Council

#### DATE VALID

14 April 2020

#### TARGET DATE

4 September 2020

#### Electoral Wards Affected:

Hunslet and Riverside

#### Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

#### RECOMMENDATION:

**For Members to note the contents of this report which is provided by way of an update to City Plans Panel on the progress of the planning applications.**

### 1.0 INTRODUCTION:

1.1 This is a joint update report for the applications for planning permission and listed building consent, for alterations and improvements to New Station Street, Bishopgate Street, Neville Street and Dark Neville Street to create a fully accessible multi modal transport hub that connects key areas of the city, promotes the use of sustainable travel opportunities (walking and cycling) and improves links to the wider City Region.

- 1.2 City Plans Panel considered these proposals on the 16 July 2020 and resolved that Application No. 20/02048/FU be deferred and delegated to the Chief Planning Officer for approval subject to the conditions specified in the submitted report (and any others which the Chief Planning Officer considers appropriate) and following further discussions with the developer concerning access issues, in particular whether or not a ramp could provide access to the taxi rank on Bishopgate Street from New Station Street and that Application No. 20/01996/LI (Listed Building Consent) be deferred and delegated to the Chief Planning Officer for approval subject to the conditions specified in the submitted report.
- 1.3 As advised in the report of the Chief Planning Officer of 16 July 2020 (which can be found at Appendix 2) Leeds City Station is the busiest in the North of England and of national importance. Passenger numbers are projected to grow by 81% by 2043. HS2, Northern Powerhouse Rail (NPR) and major upgrades to Trans-Pennine routes and services drive these capacity ambitions. By redeveloping the Station to accommodate these services, there is an opportunity to create a gateway that drives the economic growth and regeneration of Leeds City Centre and the Leeds City Region. Central to this project is the development of a fully accessible multi-modal transport hub, accessible to all, connecting key areas of the city, promoting the use of sustainable travel opportunities and improving links to the wider City Region. This will be achieved by facilitating a smooth transition between rail travel and onward pedestrian, cycling, taxi and bus routes.
- 1.4 The scheme is brought back to City Plans Panel to report on the progress made since the 16 July 2020 Panel meeting, in addressing Members request for further discussions to be undertaken regarding access issues and the potential for a ramped access route between New Station Street and Bishopgate Street, instead of the proposed lifts and steps, to ensure that all potential options have been fully examined.
- 1.5 In addition, following City Plans Panel of 16 July 2020, Officers have again reviewed the draft Conditions with the Applicant, to ensure they are robust and meet the tests for Planning Conditions as outlined in Paragraph 55 of the National Planning Policy Framework – that is, that they are (1) necessary, (2) relevant to planning, (3) relevant to the development to be permitted, (4) enforceable, (5) precise and (6) reasonable in all other respects. The updated draft Conditions are set out in full in Appendix 3 and the amendments are detailed in the appraisal below.

## **2.0 PROPOSAL:**

- 2.1 The principle aim of the proposal is to create a fully accessible multi modal transport hub with free flowing pedestrian movement out of the station to the city centre and wider city areas. The new pedestrian focused arrival space to the front of Leeds Train Station will reduce the flow of vehicular traffic, improve connectivity and create a safer environment for pedestrians and cyclists using the station and adjacent streets. Further to this, the scheme also aims to respond to counter-terrorism requirements to protect the New Station Street station entrance from potential vehicle incursion, be this deliberate or accidental, and to make the space safer for the larger number of people that will be using it. The scheme also aims to improve air quality by discouraging general traffic and by creating connections to the existing cycling routes and improving route connectivity.
- 2.2 The public benefits to the scheme include:

1. The provision of a state of the art multi-modal transport interchange.
2. The promotion of sustainable modes of transport.
3. Improved pedestrian routes along New Station Street from the junction with Boar Lane to the point where it meets City Square and connecting the station to Bishopgate and route to the south and east of the city centre.
4. An improved environment with reduced traffic levels in the area
5. An enhanced entrance for the station and defined gateway to the city.
6. Accessible connections to the station from Bishopgate via the new pedestrian lifts.
7. The creation of a new purpose built taxi shelter, with a larger waiting area than the current provision, better access arrangements for getting in and out of waiting taxis and covered seating.
8. Improved, defined and connected cycle routes.
9. Improved and expanded cycle parking provision, encouraging the use of this sustainable mode of transport.
10. Green landscape features including trees to create an attractive, environmentally positive area outside the station entrance.
11. New level high quality paving, creating level accessible attractive streets and spaces.
12. Environmental enhancements to the area under the Neville Street Bridge and along Dark Neville Street to make these places feel safer and more attractive to users.

2.3 The full details of the components of the scheme are set out in Sections 2.0 and 10.0 of Appendix 2. However, a key component of the scheme is the proposal to create two new 21 person capacity pedestrian lifts and multiple sweeping steps connecting Bishopgate and the station entrance and it is these elements that Members have requested be explored further in respect of whether or not the lifts could be replaced with a ramp.

### **3.0 SITE AND SURROUNDINGS:**

3.1 The site is the existing entrance area and transport interchange to the front of Leeds Train Station, as well as the carriageway and footways to Bishopgate Street and Neville Street (under the bridge), and an area underneath the existing interchange. The areas in question are currently trafficked with a variety of vehicles able to traverse them. There is an existing taxi rank running along New Station Street and in to the interchange area, which is adjacent to an existing cycle hub.

3.2 The site is partly within the boundary of the City Centre Conservation Area and within the setting of the Grade II Listed Queens Hotel, 40 Boar Lane (O'Neill's public house), the Scarbrough Hotel public house, The Griffin Hotel, Nos 4-10 Mill Hill, 3 The Bourse, 58, 59, 60, 61-62, and 63 Boar Lane, Leeds Train Station Concourse and Railway Company Offices, the General Post Office, the former Majestic Cinema, listed K6 telephone kiosks lamp standard statues curving symmetrically either side of the Black Prince statue, and the Grade II\* Listed statue of the Black Prince in City Square, Mill Hill Chapel and its boundary wall (grade II listed).

### **4.0 RELEVANT PLANNING HISTORY:**

4.1 None

### **5.0 HISTORY OF NEGOTIATIONS:**

- 5.1 The proposals have been the subject of pre-application discussions since October 2019. These discussions focused on scale, massing and design, new surfacing treatments, opportunities for green landscape features, the changes to vehicle movements and the promotion of sustainable modes of transport and accessibility (with the Access Officer being involved in and supporting the proposals throughout both the pre-application and the application processes).
- 5.2 The proposals were presented to Members at Plans Panel on 30 January 2020 at pre-application stage and again for determination on 16 July 2020. Members made the following comments at the Plans Panel of 16 July 2020:
- The vast majority of Members were supportive of the need to address the current issues with the station and the positive environmental and sustainable aspects of the scheme but recognised further discussions were required in terms of level access to the taxi rank.
  - Some Members expressed disappointment at the possible lack of engagement with Disabled Groups, the lack of consideration of the appearance and design of the new entrance, the lack of a level access to the taxi rank, and the adverse impact on bus services. Some Members therefore were not prepared to support the proposal.
  - Councillor Nash strongly objected to provision of a taxi feeder rank on Concordia Street.

## **6.0 RELEVANT PLANNING POLICIES:**

- 6.1 All relevant Policies can be found listed in Section 8.0 of Appendix 2.

## **7.0 MAIN ISSUES**

1. Accessibility and Inclusivity
  - The Existing Taxi Rank
  - The Proposed Bishopgate Street Taxi Rank
  - The Proposed New Lifts
  - The Proposed New Steps
  - Ramp Options
  - Option 1 Northern Ramp.
  - Option 2 Southern Ramp
2. Consultation
3. Other Matters

## **8.0 APPRAISAL**

### **8.1 Accessibility and Inclusivity**

- 8.2 The proposals include the moving of the existing taxi rank to a new purpose built stand on Bishopgate Street. Concerns were raised by Objectors to this due to the increased distance of travel to the taxi rank (an extra 15 metres) and that this would have to be via 2 new 21 person pedestrian lifts or via a number of sets of steps between New Station Street and Bishopgate Street. City Plans Panel Members asked (on 16 July 2020) that there be further discussions with the Applicant concerning access issues with both Officers and Access Representation Groups, in particular the provision of level access to the taxi rank on Bishopgate Street from New Station Street and that the option of whether or not a ramp, instead of the lifts and steps, or a rank on New Station Street, was possible.

### **8.3 The Existing Taxi Rank**

The existing surfacing arrangement along New Station means that approximately 55% of the full street is dedicated to pedestrian space, with the remainder being a vehicular route. It is worth reiterating that the current taxi rank on New Station Street operates with the following operational and accessibility issues;

- To reach the current taxi rank from the station there is a need to cross vehicular lanes which carry taxis, buses, emergency vehicles and delivery vehicles.
- The current route, although recently improved, still requires people to navigate a narrow footway which runs along the frontage of the existing Cycle Point building, with vertical street furniture located in the pedestrian route, further narrowing the available space.
- To get to the existing taxi rank from Bishopgate Street, users must navigate to either end of New Station Street (City Square or Boar Lane ends), or use the partially enclosed existing staircase.
- The existing rank's loading is from the driver's side of the taxi. Wheelchair accessible taxis are generally designed to be loaded from the passenger's side or the rear. As a result, the taxi rank has a specific crossing point to an island to enable boarding of wheelchair users from the right or the rear, however this is not at the front of the queue.
- There is an existing level pedestrian route leading from the station entrance to both this taxi rank and to City Square under the portico of the Queens Hotel.

#### 8.4 **The Proposed Bishopgate Street Taxi Rank**

As set out in Section 10.0 of the Panel report of 16 July 2020 (see Appendix 2), there are a number of improvements, in the originally designed scheme, that result from the relocation of the taxi rank to Bishopgate Street. The scheme, as originally designed, has been developed to take into account the Pedestrian Comfort Guidance for London methodology developed for Transport for London (which has become a recognised industry standard document since its introduction in 2010) and the following inclusive design good practice guidance:

- BS8300:2018, Design of an accessible and inclusive built environment – Part 1 External Environment – Code of practice.
  - BS8300:2018, Design of an accessible and inclusive built environment – Part 2 Buildings – Code of practice,
- as well as Building Regulations 2010, Approved Document M V2, 2015 edition.

The key improvements are;

- The new taxi rank will be a large purpose built shelter
- The existing taxi rank accommodates 3 vehicles with a feeder rank set out for 25 vehicles along the length of New Station Street. By contrast the new taxi rank on Bishopgate Street can accommodate up to 6 taxis and is to have a series of successive feeder ranks, with space to accommodate up to 26 spaces (32 spaces in total).
- The proposed rank is designed to load from the passenger and rear side of the taxi and as such has multiple points of entry to allow for such loading. This means less pressure for people who require assistance to access the taxi, as vehicles behind the front vehicle can load and depart without having to wait for the assistance-required taxi to depart first.
- A direct straight-line route, demarcated in a contrasting colour from the surrounding surface will be set out from the station entrance to the rank on Bishopgate Street. Clear directional signage and wayfinding will be installed on New Station Street, Bishopgate Street and within the northern and southern concourse areas of the station, to be designed in consultation with the Access and Usability Group, the appointed inclusive design specialist and the Council's Economic Development Team who are responsible for wayfinding across the city centre.
- The existing level pedestrian route leading from the station entrance to City Square under the portico of the Queens Hotel is retained and enhanced.

- Access between the different levels of New Station Street and Bishopgate Street and the taxi rank is proposed to meet a range of needs. The scheme, as originally designed, is for two large passenger lifts, strategically located at a prominent location in front of the station entrance. The lifts are 21-person capacity, and are appropriate for large volumes of people, and those with luggage / wheelchair / mobility scooter / young children / pushchairs etc. By using two lifts, additional capacity is enabled and allows for the continued functioning of one lift should maintenance or closure of the other lift be necessary.
- The lifts are intended to be partially transparent, with glass to some elevations and with a modern and clean aesthetic. In addition, the lift entrances at both New Station Street and Bishopgate Street levels have been carefully located directly adjacent to the new commercial space to offer natural surveillance.
- The lift core location also forms the edge of the ambulant step route from the station entrance to the taxi rank. This allows for a physically more legible route for visually impaired people who wish to use steps to 'follow the building line' in order to navigate to the steps and rank.
- Alternatively, multiple stair options are provided. It is the case that passenger numbers are increasing. Therefore, providing substantial areas of steps alongside lifts is required to allow the projected increases to be catered for. As such, the stairs are proposed in a broad sweep, to accommodate large numbers of people at busy periods.
- One of the stair flights accommodates a bike ramp which allows cyclists to push their bike up or down the stair flight. Its location has been selected to reduce conflict with users of the stairs who may require assistance / handrails / the most direct ambulant route between the station entrance and taxi rank.
- The proposed taxi rank shelter itself includes seating (for 15 people), together with seating in the public realm at New Station Street level. Although the route between rank and station is further, the seating and canopies in the shelter and at the lifts offers mid-points for people to rest or break the route into smaller sections.

## 8.5 **The Proposed New Lifts**

- The two new lifts are to be designed to meet the safety and accessibility requirements of BS8300-2:2018 and BS EN 81-70:2018.
- The two lifts will be large enough to accommodate 21 people in each lift.
- Call and control buttons are to have tactile indicators.
- Visual and audible indicators will also be installed to advise users that lifts are moving and/or have arrived. Audible announcements will also advise users when the lift doors are opening or closing.
- Entrance doors to the lifts will be slow opening, and will include glazing to allow users to see into and out of the lifts. The sides of the lift will be solid to allow for the location of the lift control panels.
- Each lift will be fitted with internal handrails.
- Canopies will be provided at the lift entrances to provide shelter whilst users are waiting.

## 8.6 **The Proposed New Steps**

- The new steps will be constructed with risers and goings of dimensions to meet best practice and flights will be aligned to building lines to aid detection for people with visual impairment.
- Landings are to be provided as rest points at regular intervals on the stairs.
- Tactile paving is to be installed to the tops and bottoms of all steps.
- Steps are to be treated with contrasting nose edgings.
- Left and right handrails are to be provided for the steps and to incorporate downlighters to illuminate the steps.

- The steps would have generous widths to reduce perceptions of confinement in small spaces (this is a significant upgrade over the current covered stair rotunda).
- The stairs are arranged in multiple distinct flights. This allows each separate flight to maximise accessibility by following a straight-line geometry. None of the stairs are curved or tapered, and the user is able to navigate the stairs and handrails at a 90 degree angle from the adjacent footways, minimising the risk of overstepping or miscalculating the direction of stair flights.

- 8.7 Following Plans Panel of 16 July 2020, and in response to Member's comments regarding the exploration of all access options, the Applicant has re-examined whether or not a ramped option could be utilised instead of the lifts and steps, for two potential locations along the boundary between New Station Street and Bishopgate Street. These have been considered against the British Standard (BS8300:2018) guidance, Part M of the Building Regulations and the Pedestrian Comfort methodology referred to in paragraph 8.4 above. The full report (titled MBARC Bishopgate Interchange and Access Options – Response to Leeds City Council Plans Panel) on these options is provided in Appendix 1. However, in addition, the outcomes for each option are summarised below.
- 8.8 **Ramp Options**  
Building Regulations Part M states that there should be “an alternative means of access for wheelchair users, e.g. a lift, when the total rise is greater than 2 metres”. In this case, Bishopgate Street slopes considerably. This results in a levels change between it and New Station Street of a range from approximately 1.2m to 3.7 metres. As such it is considered that lifts and steps would still be required, should a ramp be provided.
- 8.9 There are a number of existing, retained physical site constraints. These include the Mill Goit structure and its wall, the extent of property belonging to the Queens Hotel (beneath New Station Street), the space required for the new lifts and stair provision, the existing retaining wall with windows providing light to the Queens Hotel, and the existing entrance to the Queens Hotel for loading and goods. This has meant that that only two locations could be considered for a ramp.
- 8.10 **Option 1 Northern Ramp.**  
This would be to provide a ramp sited in a northerly location on Bishopgate Street, to the outer side of the Queens Hotel portico, close to City Square. The levels change here would mean only 4 separate slopes to the ramp (totalling some 24 metres in length) would be needed. Due to the gradients on Bishopgate Street these ramps would need to be 1:15 slopes.  
A ramp in this location would;  
  - Constrain and narrow the public footway on Bishopgate Street to an unacceptable level (less than 1.8 metres in width at the pinch-point).
  - Impact and reduce the amount of the Bishopgate Street public realm.
  - Significantly impact on the setting of and appearance of the portico to the Grade II Listed Queen hotel.
  - Require additional utility relocations.
  - The ramp would sit adjacent to a slightly longer but gentler route around the portico (the existing route under the overhanging section of the Queens Hotel). In this position and at 24 metres, the ramp would reduce user's journeys by 8 metres only. Therefore, although longer by 8 metres, the gentler, less convoluted route under the portico may be preferred by users.
- 8.11 **Option 2 Southern Ramp.**

This would be a ramp positioned to the south of the Queens Hotel's loading entrance on Bishopgate Street. The ramp would also need to be positioned to allow for the required lifts and steps adjacent to it. Due to the higher levels change at this point of the street 6 separate slopes, each of 10 metres in length, with gradients of 1 in 20 would be required for the ramp. Two versions of this have been explored, one with separate steps and the other with integrated steps (similar to the approach taken at the public realm adjacent to the Playhouse).

In both versions, with provision of a ramp in this location:

- The ramp would need to be 105 metres long, including 60 metres of 'ramped' surface (80 m including the turns required on the landings). If a user chose instead, to travel north along New Station Street and then south on Bishopgate Street to reach the taxi rank, then the distance would be some 130 metres.
- The ramp would mean the removal of the majority of the steps, leaving only one flight and therefore considerably reduce the pedestrian flow capacity.
- The ramp would occupy a much more significant footprint at Bishopgate Street than the proposed steps.
- The public footpath would be constrained and narrowed on Bishopgate Street to an unacceptable level and a pinch-point would be created as a result (i.e. to less than 1.8 metres which is the minimum width to allow two wheelchair users to pass).
- At the required length the ramp (i.e. 6 slopes each of 10 metres length) could be considered to be unacceptably demanding for wheelchair users and some people with walking difficulties, even if a number of rest points/ landings are provided.

8.12 The MBARC Bishopgate Interchange and Access Options – Response to Leeds City Council Plans Panel report concludes that, for the above listed reasons, neither of the possible ramped options are physically suitable in adequately addressing accessibility or meeting the aspirations to create a larger, safe pedestrian focused area on New Station Street to respond to the predicted increases in passenger numbers.

8.13 It is therefore considered that the scheme, as originally designed with the 2 pedestrian lifts, adjacent stepped access and a level route under the Queens Hotel portico providing access to the relocated taxi rank still represents the best approach to embedding accessibility into its layout and design and through its external spaces. More specifics on the elements of the accessibility features of the scheme will come through in the detailed stages under Planning Conditions submissions to ensure that the scheme does come forward in as an accessible and inclusive manner as is possible.

8.14 Consultation

8.15 The Applicant has advised that they started their public consultation in December 2019, prior to the submission of the planning application. Since that consultation commencement date and throughout the pre-application period and the application process (with the application being validated on 7 April 2020), they advise that they have consulted with the Access and Use-Ability Group, Network Rail, Leeds Chamber of Commerce, The Queens Hotel, senior Councillors, Ward Members, Train Operator Companies, Leeds Civic Trust, Rt. Hon. Hilary Benn, the Cycle Forum Sub Group, Bus Operators, Leeds BID, British Transport Police, West Yorkshire Police, Expert Panel, Leeds and Bradford Airport as well as holding public consultation events at the station and providing a 'Your Voice' survey via the West Yorkshire Combined Authority's website.

8.16 Following City Plans Panel of the 16 July 2020 the Applicant team has sought to continue consultation with key stakeholders. In respect of access and inclusivity in

particular, it has further met with the Access and Use-Ability Group (AUAG), the Leeds Disabled People's Organisation (LDPO) and with the assistance of the Access Officer, the Applicant has established and met with an 'Inclusive Design Group'. This newly formed group consists of 13 members of DAWN (The Council's Disability and Wellness network) and 7 members from external groups (Leeds Hearing and Sight Loss, Royal National Institute of Blind People and the AUAG). This is an ongoing process of consultation which the Applicant has made clear they welcome and are committed to continuing.

#### 8.17 Other Matters

- 8.18 Following City Plans Panel of 16 July 2020, Yorkshire Water provided further consultee comments on 23 July 2020 in which they withdrew their former objection and requested Conditions to cover drainage details. Such Planning Conditions are already present on the draft Conditions list.
- 8.19 As stated above the draft Conditions have been reviewed by Officers with the Applicant, to ensure they are robust and meet the tests for Planning Conditions as required under Paragraph 55 of the National Planning Policy Framework – that is, that they are (1) necessary, (2) relevant to planning, (3) relevant to the development to be permitted, (4) enforceable, (5) precise and (6) reasonable in all other respects.
- 8.20 As a result of this review, the trigger for Condition 11 on application 20/02048/FU has been amended to read “**Prior to first use of the newly laid pedestrian areas** a pedestrian wayfinding scheme, including alteration of existing signage shall be submitted to and agreed in writing with the Local Planning Authority. The approved signage scheme shall be implemented and completed before first use of the development”.
- 8.21 Condition 12 on application 20/02048/FU has been amended to read “The **kiosk and cycle hub** shall each not be occupied until provision has been made in the means of access both to and within the **said kiosk and cycle hub**, and within sanitary conveniences, for the needs of employees and members of the public who are disabled as well as non-gender specific provision. All such facilities shall thereafter be retained and made available for use”.
- 8.22 Condition 14 on application 20/02048/FU has been amended to read “a) No **landscape** work shall commence until full details of the load bearing cell type rooting zone using proprietary structures has been submitted and approved in writing by the local planning authority”.
- 8.23 Condition 20 on application 20/02048/FU has been amended to read “No **permanent** surfacing works shall take place to the hard surfaced paved and trafficked areas and street and the landscaped areas until details and samples of all surfacing materials to the said hard surfaced and landscaped area have been submitted to and approved in writing by the Local Planning Authority. The surfacing works shall be constructed from the approved materials”.
- 8.24 Condition 24 on application 20/02048/FU has been amended to read “The **taxi operational elements of the** development shall not be brought in to use until a Taxi Operations Management Plan, including details of how taxis will be called from the feeder ranks to the new taxi shelter ranks and how the ranks and taxi waiting shelter will be maintained, has been submitted to and approved in writing by the Local Planning Authority. The plan shall be fully implemented and operated in accordance with the approved timescales”

- 8.25 Condition 27 on application 20/02048/FU on application 20/02048/FU has been amended to read “Prior to the occupation of **either the kiosk or cycle hub, whichever is occupied first** details shall be provided in respect to the management, inspection and maintenance of any non-adopted drainage features. The details shall identify the responsible parties and set out how these will be funded and managed and provide a schedule of the proposed inspections and annual maintenance for the lifetime of the development. The plan shall be submitted to and approved in writing with the Local Planning Authority prior to first occupation and the development shall thereafter be maintained at all times in accordance with the approved details”.
- 8.26 These above listed Condition amendments have been added to the full draft Conditions in Appendix 3 and are also agreed with the Applicant.

## **9.0 CONCLUSION**

- 9.1 The exploration work done by the Applicant in relation to the potential other access options, as detailed above, demonstrates that none of these options would be the best answer to addressing the many demands of this scheme. As such Officers consider that, on the basis of the findings of the access options report (in Appendix 1), the originally designed scheme, to create the two new 21 person pedestrian lifts and the steps to both sides of the lift/kiosk building, and provide a level access under the Queens hotel portico would bring the optimum solution for addressing accessibility between New Station Street and Bishopgate Street.

Background Papers:  
PREAPP/19/00511  
20/01996/LI  
20/02048/FU

## MBARC Bishopgate

### Ramp Options – Response to Leeds City Council Plans Panel

17 September 2020

MBARC\_B-ATK-GEN-00-RP-LM-0002



# Notice

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This document has 20 pages including the cover. The appendices are additional to this page count.

## Document history

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Rev 1.0	For Issue	JR	TG	TG	LE	17/09/2020

## Client signoff

Client	Leeds City Council
Project	MBARC Bishopgate - Active and Sustainable Travel within Leeds City Centre
Job number	5193758
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# Introduction

This technical note has been produced in response to the City Centre Plans Panel meeting held on the 16th July 2020. At the meeting the Leeds Station Sustainable Travel Gateway project planning application was presented to Members for discussion. Members voted to defer and delegate the planning decision to the Leeds City Council Chief Planning Officer and requested that the Project Team explore the possibility of installing a ramp between New Station Street and Bishopgate Street to increase the number of step free options at the station.

The use of a ramp was considered during the initial stages of the concept design and rejected due to extensive level changes. This document, however, seeks to explore the possibility of a ramp in further detail and has identified two possible locations on Bishopgate Street, assessing the constraints and implications of each location from a usability and technical perspective.



# 1. Options for Ramp Access to New Station Street

## 1.1. Introduction

The proposals submitted for the planning consent relating to Bishopgate Street and New Station Street include a significant change in level between the two streets, in the order of three metres (this varies due to the existing slopes of Bishopgate Street and New Station Street).

The proposals for the scheme include three options which are (see **Fig 1.0**):

- Two new 21 person lifts (calculations shown that 1.14 lifts are required for station users, two lifts are provided to give additional capacity).
- Three flights of stairs, all designed to meet modern standards.
- A step free route along New Station Street and Bishopgate Street, largely following the existing gradients for these locations.

At the Plans Panel meeting on the 16<sup>th</sup> July, the project was asked to re-consider the introduction of a ramp as another option for users travelling between New Station Street and Bishopgate Street. Ramps were considered during the early design phase and rejected due to the extensive level changes which resulted in overly long ramps which were deemed unfeasible. Following Plans Panel, the design team have revisited the potential to provide ramp(s).

In section 1.2 of this technical note the key extracts of technical standards relating to vertical circulation are set out for information and reference. Section 1.3 describes and illustrates the site constraints and shows what spaces may be available for ramp construction. Section 1.4 shows outline designs for ramps, following the relevant technical guidance and the implications of such ramps.



**Fig 1.0:** Proposed Layout

## 1.2. Technical standards relating to vertical circulation (level change)

Ensuring changes in level are as accessible as possible is covered by all relevant national standards on accessibility. Below we have summarised the key aspects of each standard which has informed the design. Collectively they illustrate that ramps are not actually considered to be the most accessible option in the circumstance due to the level changes involved.

### 1.2.1. BS8300:2018-1, Design of an accessible and inclusive built environment

This is the latest guidance covering the design of the public realm published by the British Standards Institute in 2018. The standard is held in high regards and forms the basis of most UK based guidance and indeed global standards.

The most relevant aspects are set out below with key sentences emboldened for ease:

#### Clause 8.1.1 General

“Access routes should not contain steps, stairs, turnstiles, revolving doors, escalators or other features which constitute a barrier to disabled people, unless a suitable means for bypassing the barrier has been provided close by and is always available for use.

**NOTE** Many people with a mobility impairment find navigating long sloped routes challenging and therefore alternatives such as conventional passenger lifts or stairs would be beneficial, where appropriate. Recommendations for vertical movement are given in [Clause 9](#).”

#### Clause 8.1.4 Gradients

“An access route should either be level along its length or (where the topography of the land prevents this) should be gently sloping or incorporate a ramp or ramps in accordance with [9.2](#). **Where the change in level is sufficient to avoid a single step, a stepped approach should also be provided (see [9.1](#)).**

*NOTE 1 It is assumed that a gradient of 1:60 or less steep is level; steeper than 1:60 (but less steep than 1:20) is gently sloping; and 1:20 or steeper is a ramp.*

### 9.2 Ramps

#### “COMMENTARY ON 9.2

*If a change in level along pedestrian routes is unavoidable, it is necessary to provide gently sloping or ramped options. However, as **some people with an ambulant mobility impairment have difficulty using ramps, it is undesirable for a ramp to be the only route.***”

#### 9.2.2 Gradient of a ramp

“A ramp **should have the lowest practicable gradient** within the range 1:20 to 1:12 and the maximum corresponding length between landings.”

“**If a series of ramp flights rises more than 2 m, an alternative means of step-free access should be provided.** Where this is by means of a lifting appliance, it should meet the recommendations in [9.4](#).”

### 1.2.2. The Building Regulations 2010, Approved Document M V2, 2015 edition

This document is the guidance associated with the Building Regulations. Whilst it was last updated in 2015, the substance of the requirements have largely remained the same since 2005 when it was updated to take into account of the first edition of BS8300, published in 2001.

The most relevant aspects are set out below with key sentences emboldened for ease:

**Design consideration 1.3 (approach from boundary of the site)**

“It is important to be aware that people’s capabilities vary. For example, for some people, a stair is easier to use than a ramp.”

**Design consideration (approach from boundary of the site)**

“Where a difference in level between the boundary of the site or car parking designated for disabled people and the building is unavoidable due to site constraints, the approach may have a gentle gradient over a long distance (for all or part/s of the approach) or it may incorporate a number of shorter parts at a steeper gradient, with level landings as rest points. **Generally, gradients within the approach should be as gentle as possible.**”

**Design consideration (Ramps)**

“**If site constraints necessitate an approach of 1:20 or steeper**, an approach incorporating a ramped access should be provided. Ramps are beneficial for wheelchair users and people pushing prams, pushchairs and bicycles.”

**Design consideration (Ramps)**

“**Gradients should be as shallow as practicable**, as steep gradients create difficulties for some wheelchair users who lack the strength to propel themselves up a slope or have difficulty slowing down or stopping when descending.”

**Design consideration (Ramps)**

“**Ramps are also not necessarily safe and convenient for ambulant disabled people.** For example, some people who can walk but have restricted mobility find it more difficult to negotiate a ramp than a stair. In addition, adverse weather conditions increase the risk of slipping on a ramp. It is therefore beneficial to have steps as well as a ramp.

**Design consideration (Ramps)**

“**If the total rise of a ramped approach is too high, it can be unacceptably tiring for wheelchair users and some people with walking difficulties, even if a number of rest landings are provided.**”

**Provision 1.26 d) (Ramps)**

“There is an alternative means of access for wheelchair users, e.g. **a lift, when the total rise is greater than 2m;**”

### 1.2.3. Design Standards for Accessible Railway Stations, DfT, 2015

This document provides guidance specifically on Railway Stations, however it frequently refers back to BS8300:2009+A1:2010 which has since been superseded by BS8300:2018. The key requirement of this standard is set out below:

**P1 (ramps)**

“1. Some ambulant-disabled people have difficulty using ramps, **so a ramp should not be the only approach to the station.**”

“6. **No series of ramps to a building should rise in total more than 2 metres.**”

**“7. If a series of ramp flights rise more than 2 metres, an alternative means of access, such as a lift, should be provided.”**

### 1.3. Physical constraints and available space for ramp provision

Overleaf is a drawing combining the proposals with some of the key constraints overlaid to assist in understanding the physical constraints (**See Fig. 2.0**). This drawing also shows the extent of potential space to provide a ramped access between Bishopgate Street and New Station Street.

The key constraints identified are:

In pale blue:

- The existing Mill Goit Structure and its wall which is to be retained to create the cycle store beneath the public plaza as part of the proposals.
- The extent of property belonging to the Queens Hotel, located beneath New Station Street.

In yellow:

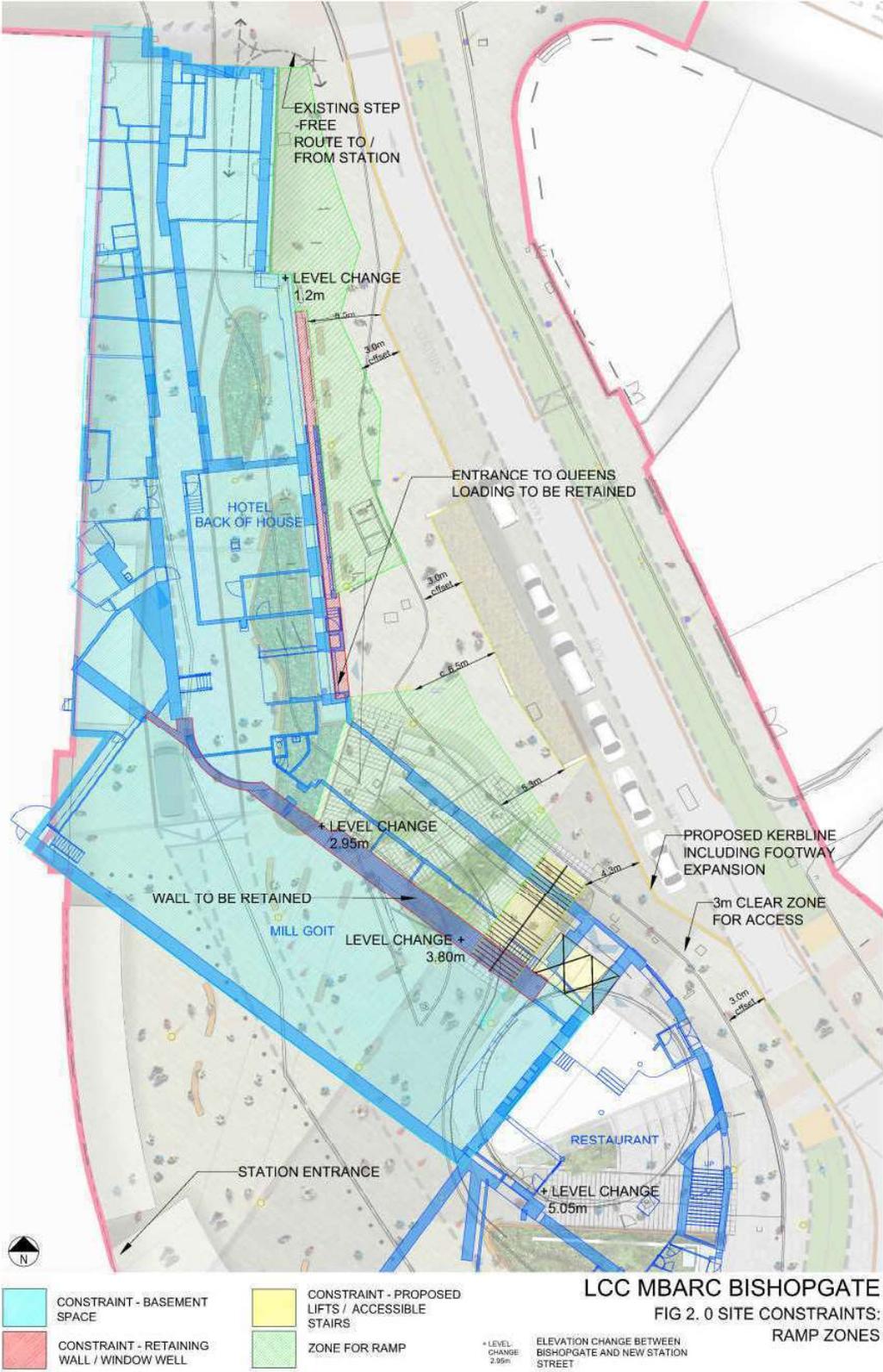
- Space required for the new lifts and stair provision (as required by all national guidance as set out in section 2.0)

In red:

- The existing retaining wall with windows providing light to the Queens Hotel's property.
- The existing entrance to the Queens Hotel for loading and goods which is to be retained.

In pale green:

- Shows two potential zones where ramps may be constructed, taking in to account the constraints and need to maintain space for the new Taxi Rank, road, cycle route and pathways on Bishopgate Street itself.



**Fig 2.0:** Site constraints for ramps

## 1.4. Compliant ramp options in context

Drawing on the technical standards in section 1.2, the drawings on the next pages show scale drawings considering the potential for ramps between New Station Street and Bishopgate. The two options are summarised below:

### North Ramp Location

In this location the level change is circa 1.2m which means that minimal total ramp construction is needed.

The example shown uses 4 separate slopes of 1 in 15, each covering 300mm change in level, which are 4,500mm long each. The ramp is 1,800mm wide to ensure there is enough width for passing of wheelchair users side by side as required by standards.

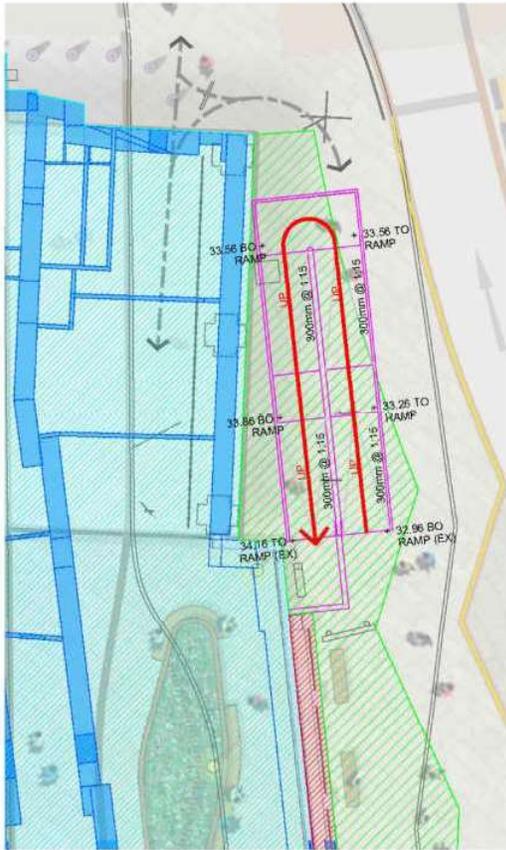
We have considered constructing a ramp using 1 in 20 slopes, to be as gentle as possible as is encouraged by the technical standards however this would be 'self defeating' in this location because the existing gradients on Bishopgate Street would be falling with the ramp. For example, two 10m long 1 in 20 slopes would arrive in a similar location to that currently shown but would still require a further slope to address the remaining 200mm in level change.

If a ramp was constructed in this location, it would:

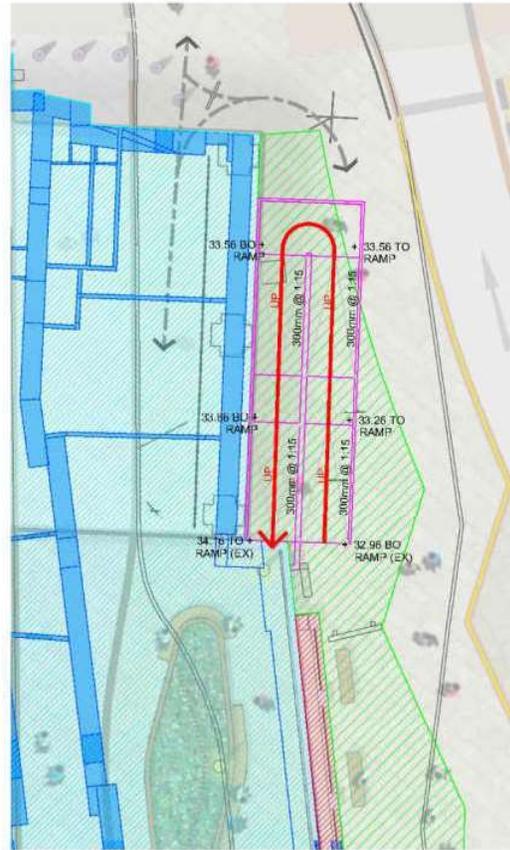
- Narrow the available footpath on Bishopgate Street to approximately 1,800mm which is the minimum width to allow two wheelchair users to pass and considerably less than the existing width at this busy location.
- Impact on the general capacity of the Bishopgate Street public realm.
- Impact the view of the side of the Portico to the Queen hotel. An additional listed building consent may be required given the changes needed to the listed wall.
- Require additional utility relocations and additional project budget. There are a number of above ground utilities in this location.

From a usability perspective, the new ramp would be in the order of 24 metres long. Therefore, the benefits of this ramp are limited as it only saves users approximately eight metres of travel distance.

From a technical standards perspective, the longer gentler route around the Portico is more accessible than the proposed ramp because it is gentler (See section 2.0, technical standards encourage the gentlest route possible with ramp construction only if necessary).



RAMP LENGTH REQ'D:  
18m + LANDINGS (25m TOTAL)



RAMP LENGTH REQ'D:  
18m + LANDINGS (24m TOTAL)

Figs 2.1 and 2.2 – Options for a ramp near the Queens Porte Cochere

## South Ramp Location

In this location the level change varies from 2,950mm nearest the Queens Hotel's loading entrance, rising up to 3,700mm adjacent the existing cycle hub. The exact level change for a ramp in this location would depend on the precise location of its top and bottom landings, and the finished floor level on Bishopgate Street which may be adjusted to a limited extent. For the purposes of this ramp study, a level change of 3,000mm has been assumed as a likely necessity. This is towards the lower limits of the range of level change. Additionally, the points used to start and end of the ramp should be toward the north of this area, close the Queens Hotel's loading entrance to minimise the extent of the ramp needed. Lifts and stairs are positioned adjacent the cycle hub and are required regardless of the provision of a ramp by the technical standards, for example Approved Document M of the building regulations states:

### Design Consideration 1.21 (Ramps)

**“Ramps are also not necessarily safe and convenient for ambulant disabled people.** For example, some people who can walk but have restricted mobility find it more difficult to negotiate a ramp than a stair. In addition, adverse weather conditions increase the risk of slipping on a ramp. It is therefore beneficial to have steps as well as a ramp.

### Provision 1.26 d) (Ramps)

**“There is an alternative means of access for wheelchair users, e.g. a lift, when the total rise is greater than 2m;”**

We have considered two potential ramp options.

**Fig 2.3** assumes that the passenger lifts and their adjacent stair flights are retained in their proposed location. The new ramp is proposed adjacent to them.

The proposal shown in **Fig 2.3** uses 6 separate slopes of 10m in length, at a gradient of 1 in 20. This is the maximum gradient and length permitted for ramps under the technical standards. The ramp would need to be at least 1.8m wide to allow for two way passing of wheelchair users.

If a ramp was constructed in this location, it would:

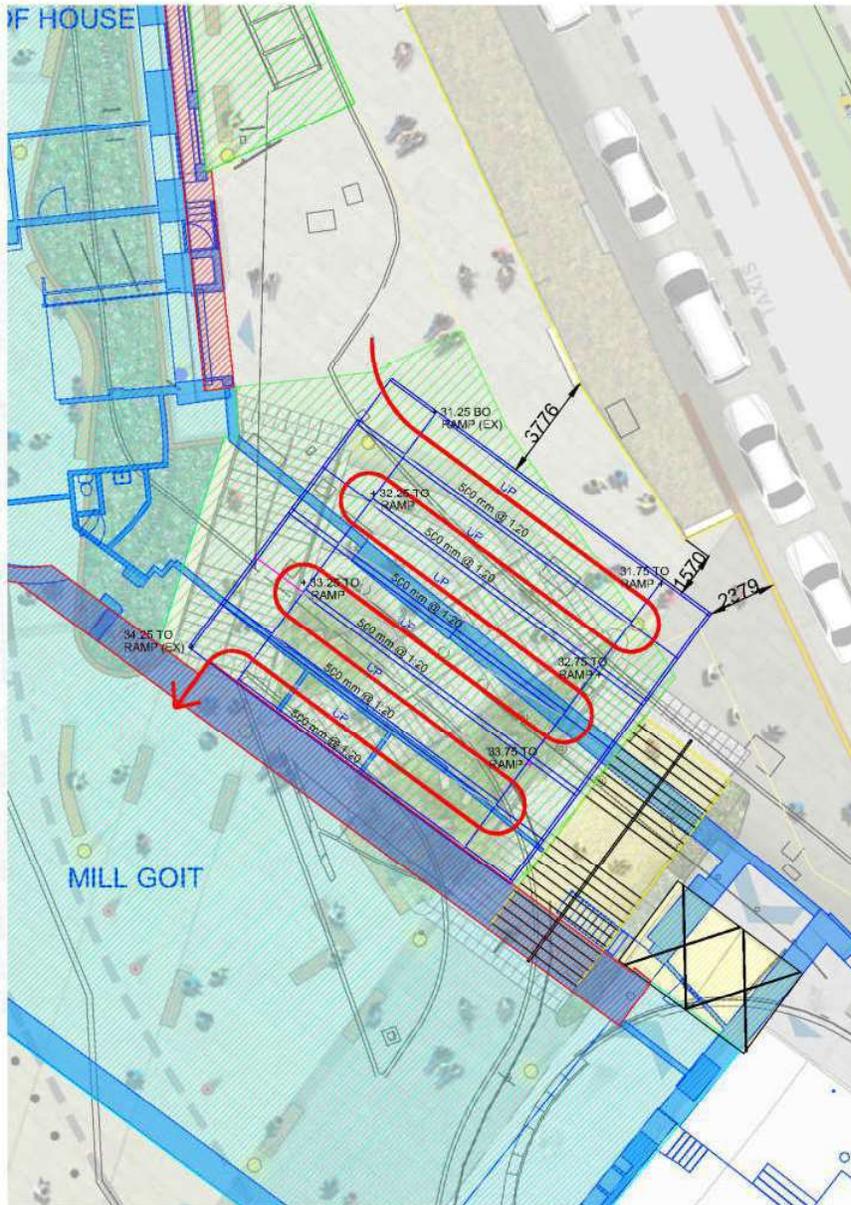
- Remove considerable flow capacity (1,800mm wide ramp vs 9,000mm or stair width lost).
- Occupy a much more significant footprint at Bishopgate Street than the proposed steps
- Narrow the available footpath on Bishopgate Street to less the 1,800mm which is the minimum width to allow two wheelchair users to pass.
- Provide an alternative route (130m in length) with one measuring 105m.

From a usability perspective, from the centre of the plaza to the taxi shelter on Bishopgate Street the ramped route would be in the order of 105 metres long, including 60 metres of 'ramped' surface (80 m including the turns required on the landings). The alternative to this would be 130 metres along New Station Street and back down Bishopgate Street. The additional distance is circa 25 metres, however the longer route is not steep enough to be a ramp, it is a shallower route which is therefore more accessible.

A second option includes for a similar ramp structure (**Fig 2.4**), but with a stair flight integrated with it which would allow people to transfer between the steps and ramp at regular intervals in a similar way as at the new Playhouse public realm. The steps are located at the opposite side of the ramp from the lifts, to utilise the lower elevation change to climb to New Station Street.

If a ramp and integrated steps was constructed in this location, it would:

- Remove considerable flow capacity (1,800mm wide ramp vs 9,000mm or stair width lost).
- Occupy a much more significant footprint at Bishopgate Street than the proposed steps
- Narrow the available footpath on Bishopgate Street to less the 1,800mm which is the minimum width to allow two wheelchair users to pass.
- Provide an alternative route (130m in length) with one measuring 105m.



**FIG 2.3 SOUTH RAMP LOCATION:**  
**OPTION 1 - RAMP TO REPLACE STEPS.**  
 STEP FLIGHT NEXT TO LIFTS RETAINED. NEW RAMP NOT INTEGRATED WITH STEPS.  
 RAMP LENGTH REQUIRED:  
 60m + LANDINGS (80m TOTAL).

Figs 2.3 – South Ramp Option 1

A second option includes for a similar ramp structure (**Fig 2.4**), but with a stair flight integrated with it which would allow people to transfer between the steps and ramp at regular intervals in a similar way as at the new Playhouse public realm. The steps are located at the opposite side of the ramp from the lifts, to utilise the lower elevation change to climb to New Station Street.

If a ramp and integrated steps was constructed in this location, it would:

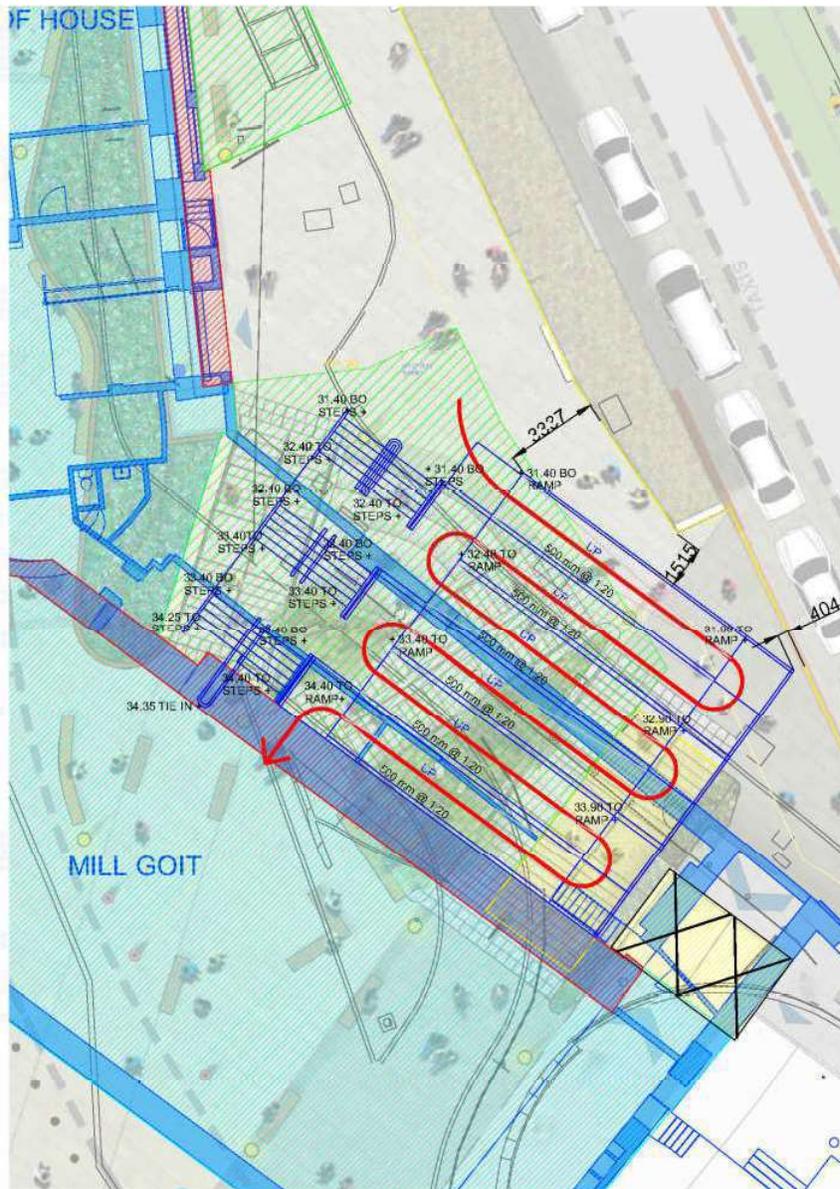
- Remove considerable flow capacity (1,800mm wide ramp vs 9,000mm or stair width lost).
- Occupy a much more significant footprint at Bishopgate Street than the proposed steps
- Narrow the available footpath on Bishopgate Street to less the 1,800mm which is the minimum width to allow two wheelchair users to pass.
- Provide an alternative route (130m in length) with one measuring 105m.

From a technical perspective, all three of the technical standards set out in section 2.0 urge caution with long ramps attempting to accommodate changes in of more than 2 metres in height. For example, Approved Document M of the building regulations states:

#### **Design Consideration 1.25 (Ramps)**

**“If the total rise of a ramped approach is too high, it can be unacceptably tiring for wheelchair users and some people with walking difficulties, even if a number of rest landings are provided.”**

In this instance the ramps would be approximately 3 metres in height. In our view there would be very few people who would choose to ascend ramps over the other options available.



**FIG 2.4 SOUTH RAMP LOCATION:  
OPTION 2 - RAMP TO REPLACE STEPS.  
NEW STEPS AND RAMP INTEGRATED**

**RAMP LENGTH REQUIRED:  
60m + LANDINGS (80m TOTAL)**

**Figs 2.4 – South Ramp Option 2**

## 1.5. Conclusions

We have considered the potential to introduce ramped options for people travelling between New Station Street and Bishopgate Street. In our view, after considering the technical standards from a range of relevant sources and having regards to the wider site constraints and project requirements the consequences of introducing a ramped option would outweigh the perceived benefits.

The option at the northern end of Bishopgate Street would restrict the public realm space available and introduce a new pinch point on Bishopgate Street for a very modest time/distance saving.

The option of a ramp towards the south would also introduce a pinch point on Bishopgate Street and would also reduce the overall capacity of the connection between the two levels significantly.

In our view, neither option of ramp offers significant benefit for disabled people when compared to the options available within the proposed scheme as submitted for planning. The longer but shallower step free route is a more accessible option than large ramp option to the south and the benefit of the ramp options to the north is minimal.

# Appendices

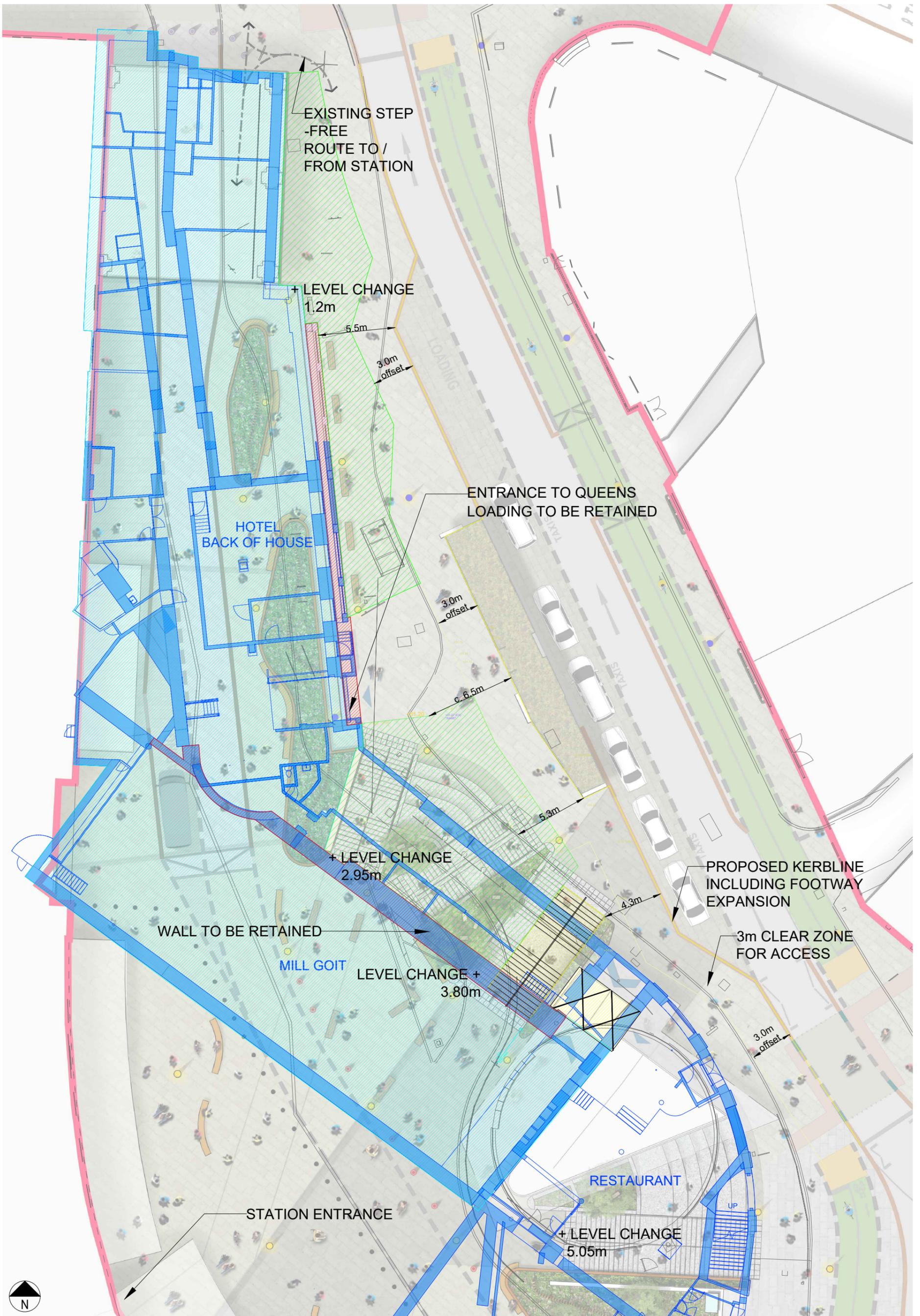


# Appendix A. Ramp Options

**Atkins Limited**  
3100 Century Way  
Thorpe Park  
Leeds  
LS15 8ZB

Tel: +44 (0)113 306 6000

© Atkins Limited except where stated otherwise



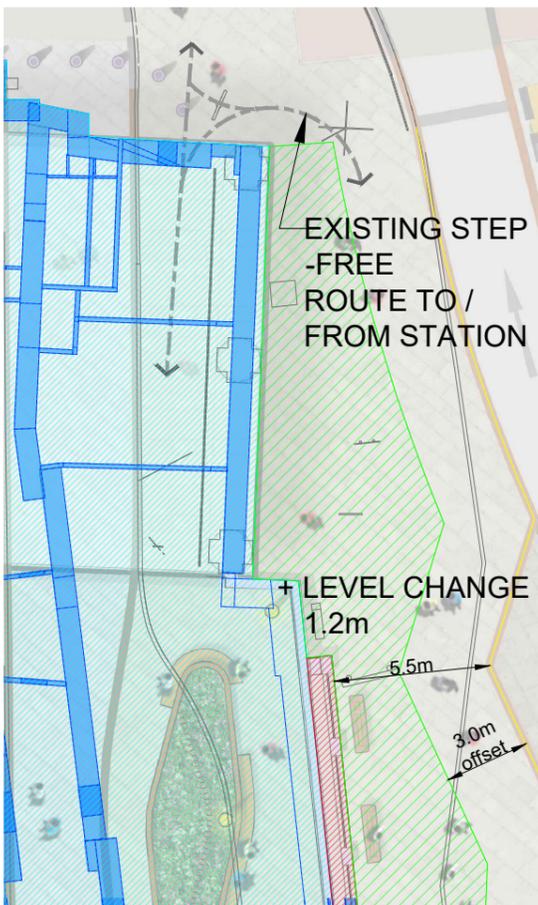
- CONSTRAINT - BASEMENT SPACE
- CONSTRAINT - RETAINING WALL / WINDOW WELL

- CONSTRAINT - PROPOSED LIFTS / ACCESSIBLE STAIRS
- ZONE FOR RAMP

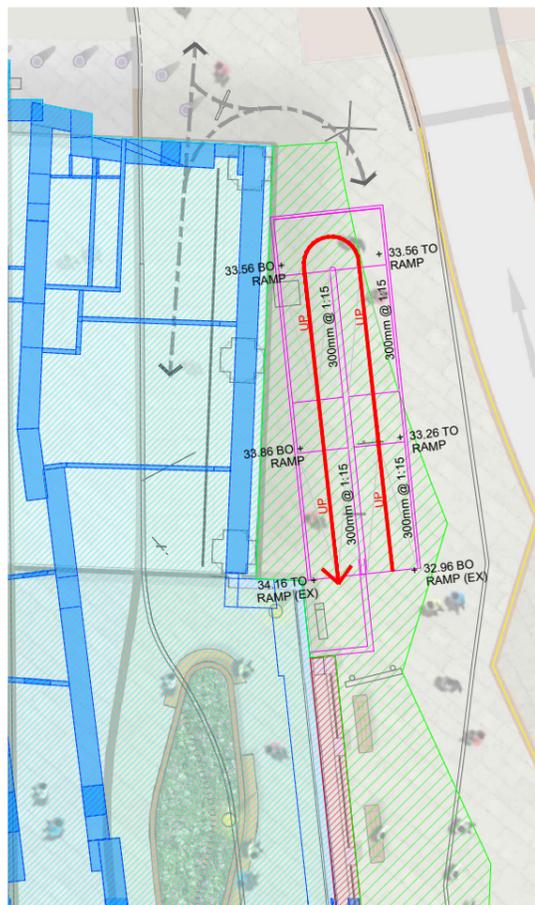
+ LEVEL CHANGE 2.95m  
ELEVATION CHANGE BETWEEN BISHOPGATE AND NEW STATION STREET

## LCC MBarc BISHOPGATE

### SITE CONSTRAINTS: RAMP ZONES

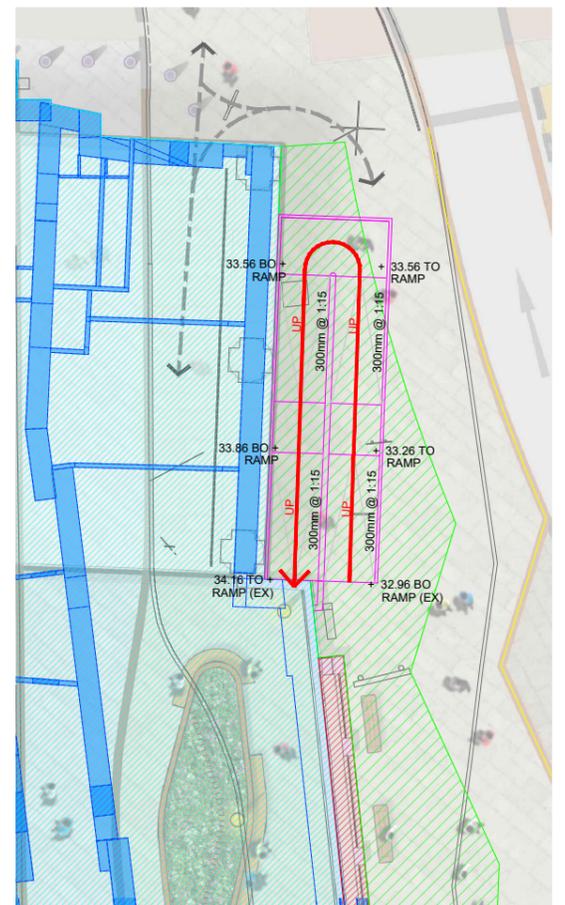


**FIG 2.2 NORTH RAMP LOCATION:  
RAMP ZONES**



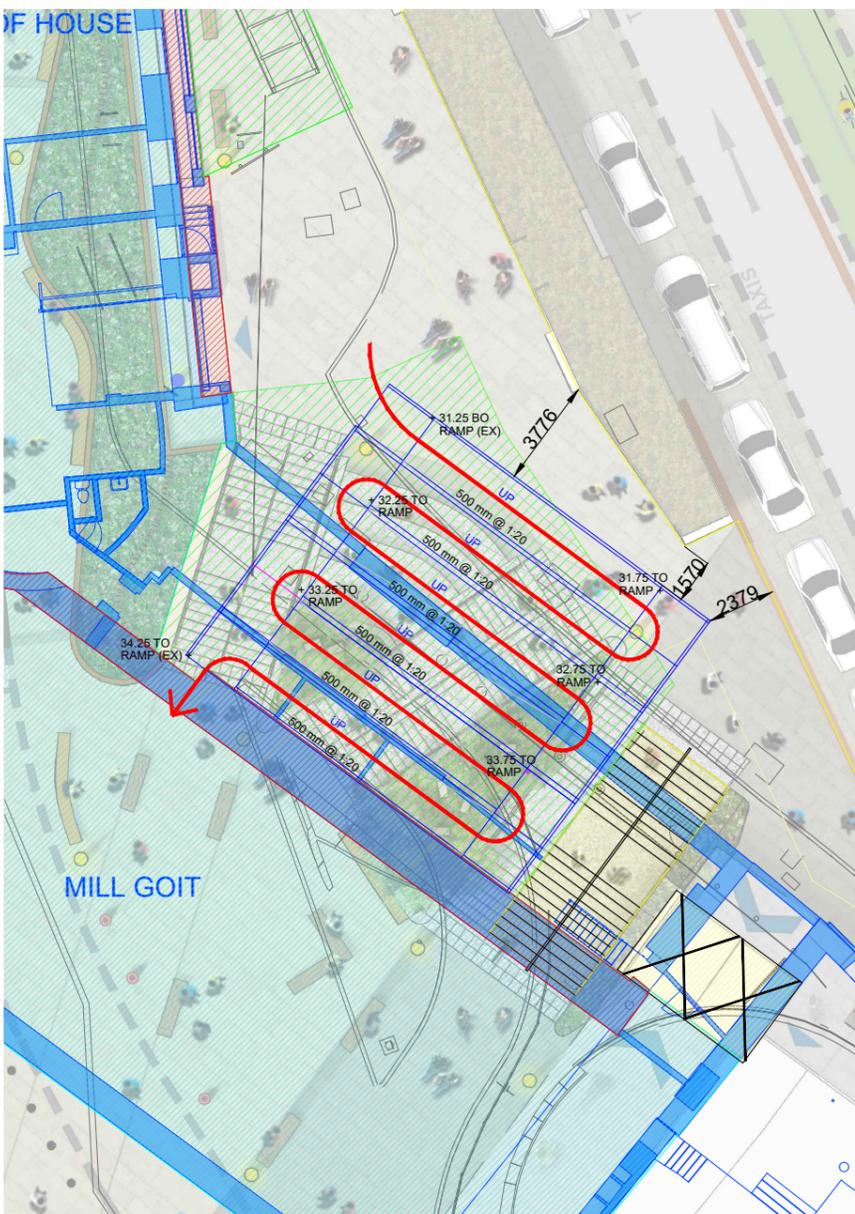
**FIG 2.3 NORTH RAMP LOCATION:  
OPTION 1**

RAMP LENGTH REQ'D:  
18m + LANDINGS (25m TOTAL)

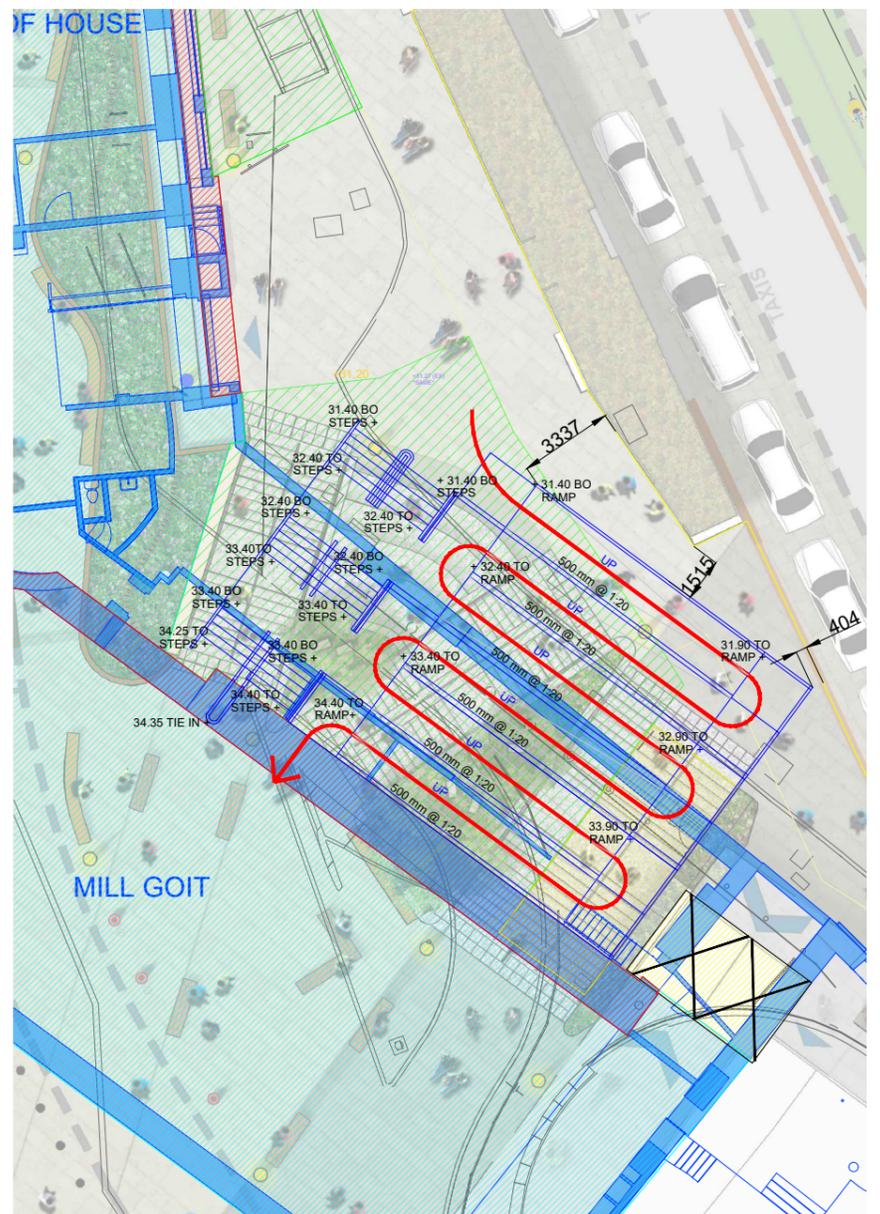


**FIG 2.4 NORTH RAMP LOCATION:  
OPTION 2**

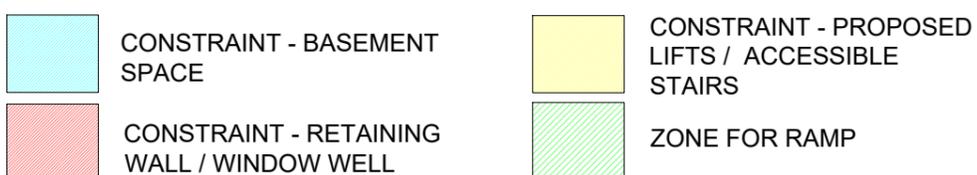
RAMP LENGTH REQ'D:  
18m + LANDINGS (24m TOTAL)



**FIG 2.5 SOUTH RAMP LOCATION:  
OPTION 1 - RAMP TO REPLACE STEPS.**  
STEP FLIGHT NEXT TO LIFTS RETAINED. NEW RAMP NOT INTEGRATED WITH STEPS.  
RAMP LENGTH REQUIRED:  
60m + LANDINGS (80m TOTAL).



**FIG 2.6 SOUTH RAMP LOCATION:  
OPTION 2 - RAMP TO REPLACE STEPS.**  
NEW STEPS AND RAMP INTEGRATED  
RAMP LENGTH REQUIRED:  
60m + LANDINGS (80m TOTAL)



+ LEVEL CHANGE 2.95m  
ELEVATION CHANGE BETWEEN BISHOPGATE AND NEW STATION STREET

**LCC MBARC BISHOPGATE  
SITE CONSTRAINTS:  
RAMP ZONES**





## Appendix 2 - Report of the Chief Planning Officer

### CITY PLANS PANEL

Date: 16 July 2020

**Subject: Planning Application 20/02048/FU for the demolition of existing structures, the construction of a new cafe and a cycle hub; a new taxi rank; public realm enhancement works including hard and soft landscaping; improvements to the cycle, pedestrian and multi-modal interchange arrangements and the installation of new access lift at Bishopgate Street, New Station Street And Mill Goit, Neville Street, And Dark Neville Street, Leeds City Centre.**

And

**Listed Building Application 20/01996/LI for the demolition of an existing section of wall and the construction of a replacement wall, which will be built into an existing listed wall at Bishopgate Street, New Station Street, Leeds.**

#### **APPLICANT**

Leeds City Council

#### **DATE VALID**

14 April 2020

#### **TARGET DATE**

7 July 2020

#### **Electoral Wards Affected:**

Hunslet and Riverside

#### **Specific Implications For:**

Equality and Diversity

Community Cohesion

Narrowing the Gap

**RECOMMENDATION: Defer and Delegate application 20/02048/FU to the Chief Planning Officer for approval, subject to the conditions at the Appendix and any others which the Chief Planning Officer considers appropriate.**

**Defer and Delegate application 20/01996/LI to the Chief Planning Officer for approval, subject to the conditions at the Appendix.**

#### Conditions

A list of draft Planning Conditions for both applications is provided in the appendix to this report.

### 1.0 INTRODUCTION:

- 1.1 This is a joint report for the applications for planning permission and listed building consent, for alterations and improvements to New Station Street, Bishopgate Street, Neville Street and Dark Neville Street to create a fully accessible multi modal transport hub that connects key areas of the city, promotes the use of sustainable travel opportunities (walking and cycling) and improves links to the wider City Region.
- 1.2 Leeds City Station is the busiest in the North of England and of national importance. Passenger numbers are projected to grow by 81% by 2043. HS2, Northern Powerhouse Rail (NPR) and major upgrades to Trans-Pennine routes and services drive these capacity ambitions. By redeveloping the Station to accommodate these services, there is an opportunity to create a gateway that drives the economic growth and regeneration of Leeds City Centre and the Leeds City Region.
- 1.3 Through the Leeds City Region HS2 Growth Strategy and the Leeds Integrated Station Masterplan (LISM), proposals are to capitalise on HS2 which will achieve a step change in rail connectivity and capacity in the North of England. This includes integrating HS2 with NPR and redeveloping the existing station in a way that maximises economic benefits.
- 1.4 The Leeds Station – Sustainable Travel Gateway project is a key component of the LISM and ensuring that economic growth is sustainable by enhancing access pathways to Leeds City Station, both within the immediate city centre area and linking into communities on the periphery of the city centre.
- 1.5 Central to this project is the development of a fully accessible multi-modal transport hub, accessible to all, connecting key areas of the city, promoting the use of sustainable travel opportunities and improving links to the wider City Region. This will be achieved by facilitating a smooth transition between rail travel and onward pedestrian, cycling, and taxi and bus routes.
- 1.6 The project is part of the Department for Transport (DfT) Transforming Cities Fund (TCF) bid being put forward by the West Yorkshire Combined Authority (WYCA) for public transport improvements across the Leeds City Region. WYCA submitted the Strategic Outline Business Case (SOBC) to the DfT on the 28<sup>th</sup> November 2019, funding announcement is expected soon.
- 1.7 The Leeds Station – Sustainable Travel Gateway project compliments a wider programme of Leeds City Council transformational transport initiatives (Armley Gyratory, Meadow Lane, Regent Street and City Square) to be delivered across Leeds that aims to reduce vehicle usage within the city centre. The proposals here submitted have been designed to allow them to compliment and co-ordinate with the wider programme and in particular the future City Square works, which will be subject an architectural design competition. It also the case that Network Rail has aspirations to undertake enhancement works to the railway stations western facing entrance (Aire Street), which they potentially aim to start designing in the autumn of this year. The works are designed to encourage a modal shift towards sustainable forms of transport by creating user friendly spaces that support opportunities for cycling, walking and connectivity to other forms of transport.

1.8 The scheme is brought to City Plans Panel as the proposals involve a major investment on a significant site adjacent to the City Centre train station, this being a major transport hub for Leeds.

## 2.0 PROPOSAL:

2.1 The principle aims of the proposal is to create a fully accessible multi modal transport hub with free flowing pedestrian movement out of the station to the city centre and wider city areas. The new pedestrian focused arrival space to the front of Leeds Train Station will reduce the flow of vehicular traffic, improve connectivity and create a safer environment for pedestrians and cyclists using the station and adjacent streets. Further to this the scheme also aims to respond to counter-terrorism requirements to protect the New Station Street station entrance from potential vehicle incursion, be this deliberate or accidental and to make the space safer for the larger number of people that will be using it. The scheme also aims to improve air quality by discouraging general traffic and by creating connections to the existing cycling routes and improving cycling connectivity.

2.2 The proposal to redevelop Bishopgate will constitute the following key components:

- Development of a multi-modal transport interchange.
- Pedestrianisation of New Station Street from the junction with Boar Lane to the point where it meets City Square, with only service vehicles and British Transport Police vehicles allowed access
- Major improvements to the main entrance, enhancing the image of the station and creating a gateway to the city.
- Complete re-design and re-building of the station entrance connecting Bishopgate and key pedestrian access routes to the east and south of the city centre.
- Accessibility improvements through the installation of two new 21 person capacity pedestrian lifts connecting Bishopgate and the station entrance.
- Enhanced pedestrian and cycle friendly routes through Neville Street, Dark Neville Street and Victoria Road to actively promote sustainable forms of transport.
- Improving connectivity across the city centre by installing high quality cycle lanes on key strategic gaps in the city centre.
- Installation of the 700 storage cycle hub at the junction of Bishopgate Street with the Neville Street Bridge providing connectivity from the Station to the cycle routes around Leeds centre and wider district. The cycle hub will include space for electric bikes and is expected to offer maintenance and repair services in addition to storage facilities.
- Infrastructure improvements will help support the Climate Emergency agenda by encouraging uptake of sustainable forms of transport.

The proposals would be designed and detailed to set a high standard and a benchmark for cohesion with future plans (which are outside the remit of this current scheme) for the closing of City Square to general traffic, enabling the reduction of through traffic in Leeds City Centre and linking through to infrastructure improvements to the M621 and Armley Gyratory and the South Bank, as stated in the Leeds Transport Strategy.

- 2.3 As such the existing retaining wall to the train station forecourt, the existing cycle hub and rotunda staircase are to be demolished. These structures are not Listed Buildings, apart from a section of the wall which is attached to Grade II Queens Hotel. Therefore the listed building application is only in relation to that section of the wall that ties into the Listed Queens Hotel. In their place would be created an upgraded plaza along New Station Street, leading from the station entrance to Bishopgate Street via a series of grand sweeping steps and two generously sized passenger lifts.
- 2.4 As stated above New Station Street would be pedestrianised along its full length from its junction with Boar Lane to the point where it meets City Square. This means that the existing taxi rank (which is situated within the bus interchange area to the front of the train station) will be relocated at the bottom of the new sweeping steps and lifts on Bishopgate Street, where a new fully accessible taxi pick up point will be located. There are proposals to include additional taxi stacking ranks on Swinagate, Concordia Street and Sovereign Street.
- 2.5 Options are being explored for buses which currently use the interchange at the train station and Bishopgate Street to potentially be re-routed to run along Boar Lane and route around the outer edges of City Square. City Square is itself part of a linked, but separate, scheme to upgrade and further traffic calm the Square.
- 2.6 To Bishopgate Street, as well as the aforementioned relocated taxi rank and removal of bus routes, the footways will be widened and a segregated two way cycle route will be introduced. This will result in the carriageway being reduced to one lane and will link in to similar works to improve cycle and pedestrian facilities along Neville Street under the Neville Street bridge. On Neville Street this will result in wider footways, a segregated two way cycle path and the carriageway reduced to one lane in each direction. The current panelling to the walls of Neville Street is to be removed and new wall mounted architectural screens and lighting is to be introduced to improve the environment for pedestrians and cyclists.
- 2.7 Proposals are also intended to Dark Neville Street, where lighting will be upgraded, vacant arches will be screened with architectural panels and new surfaces laid for defined cycle routes and for pedestrian areas.

### **3.0 SITE AND SURROUNDINGS:**

- 3.1 The site is the existing entrance area and transport interchange to the front of Leeds Train Station, as well as the carriageway and footways to Bishopgate Street and Neville Street (under the bridge), and an area underneath the existing interchange. The areas in question are currently trafficked with a variety of vehicles able to traverse them. There is an existing taxi rank running along New Station Street and in to the interchange area, which is adjacent to an existing cycle hub.
- 3.2 The site is partly within the boundary of the City Centre Conservation Area and within the setting of the Grade II Listed Queens Hotel, 40 Boar Lane (O'Neill's public house), the Scarbrough Hotel public house, The Griffin Hotel, Nos 4 through to 10 Mill Hill, 3 The Bourse, 58, 59, 60, 61-62, and 63 Boar Lane, Leeds Train Station Concourse and Railway Company Offices, the General Post Office, the former Majestic Cinema, listed K6 telephone kiosks lamp standard statues curving symmetrically either side of the Black Prince statue, and the Grade II\* Listed statue of the Black Prince in City Square, Mill Hill Chapel and its boundary wall (grade II listed).

#### **4.0 RELEVANT PLANNING HISTORY:**

4.1 None

#### **5.0 HISTORY OF NEGOTIATIONS:**

5.1 The proposals have been the subject of pre-application discussions since October 2019. These discussions have focused on scale, massing and design, new surfacing treatments, opportunities for green landscape features, the changes to vehicle movements and the promotion of sustainable modes of transport and accessibility (with the Access Officer being involved in and supporting the proposals).

5.2 A preapplication presentation for the scheme was given to Members at Plans Panel on 30 January 2020. Members made the following comments:

- This would be a very demanding scheme, make sure you plan for unintended consequences.
- A number of Members welcomed the provision of a large cycle hub, suggesting it was a really positive step forward
- The proposals for the new station were welcomed but the whole area needs to be looked at and the proposals understood within this wider station context.
- Members supported the emerging building design and proposal to remove the wall to Bishopgate Street
- Members supported the emerging landscape scheme
- Members supported the approach to traffic calming and pedestrian/ cycle improvements subject to a detailed Transport Assessment and subject to more information on wider proposals for the station area.

5.3 Ward Members were consulted on 4 May 2020. No response have been received to date.

#### **6.0 PUBLIC/LOCAL RESPONSE:**

6.1 Two letters of support received (15 April 2020 and 23 April 2020). The comments made are summarised below;

1. Fantastic to see segregated cycleway in Dark Neville Street/Dark arches
2. Could electric charging points be included toward the back of the "taxi charging" zone on Swinegate?
3. It would be useful to provide fully segregated cycleways on New Station St in line with the rest of the scheme.
4. Could infrastructure for electric charging be laid under the pavement at the same time to save extensive digging up later?
5. Could the taxi shelter be provided on the opposite side of the road or shifted east allowing the cycle lane to remain continuous without switching sides?
6. Could free standing fixed bike pump and tool stations be provided outside cycle hub for quick access or out of hours access?
7. Provision is required for Leeds visitor maps and wayfaring/navigation points.
8. There are three central sets of steps but there seems little advantage to the middle set of the three. Could something more inventive be done with that space?
9. Is Mill Hill closed to all but buses and taxis?

##### Responses:

- Point 3, 5 and 8 will be addressed in the appraisal below.
- In respect of points 2 and 4 the Applicant has stated that the initial consultation for the project has not identified a requirement for electric charging points at the station,

however, this will continue to be reviewed as the project progresses. Leeds City Council, however, are delivering a rapid charge network across the region that will provide at least 88 rapid chargers in West Yorkshire. In Leeds there will be a minimum of 30 spread around the city area. There are currently 8 live charge points in Leeds. The rapid chargers, which are dual units and can support both fast & rapid charge at AC/DC, all have a dedicated bay for Taxi & Private Hire use, with a 2nd bay for public use (though a plug-in Taxi & Private Hire vehicle could still use it if the Taxi & Private Hire bay was already in use).

- In respect of point 3, space requirements and limitations and the wish to make this a pedestrian focused area have meant it has not been possible to incorporate segregated cycleways along New Station Street.
- In respect of point 6, the Applicant states that they will look to incorporate a fixed bike pump and tool station on New Station Street.
- Re point 7, wayfinding will be subject to a Planning Condition.
- Re Point 9. Mill Hill has business that requiring servicing and as such will remain open to through traffic. Stopping restrictions on the street are likely to be reviewed by Highways to ensure that traffic flows freely.

6.2 Letters of objection received on 6 May (1 letter), 21 May (1 letter), 26 May (1 letter), 8 June (1 letter), 13 June (1 letter) ,14 June (1 letter), 15 June 2020 (1 letter), 16 June 2020 (5 letters), 17 June (1 letter) and 19 June (1 letter).

The comments made are summarised below;

1. That the plan does not enhance the integration of public transport or help meet the council target of doubling the number of bus passengers, because the bus stops currently in front of the Station south entrance are being removed to locations further away, with particular concerns regarding city centre bus 5 route and the airport bus, as well as all the other routes affected. That the loss of the train-bus interchange does not create a sustainable travel gateway and that east-west buses on Boar Lane should use New Station Street into the station area before exiting into City Square.
2. Private cars are still going to have access to the entrances off the north concourse, causing congestion at the entrance to Aire Street, and it is this access that needs to be moved away from the Station due to nuisance and air pollution issues and that it is at this entrance that a taxi rank should be formally provided.
3. The increased walking distances will make it difficult for people to locate their bus connection.
4. That the scheme would exclude and discriminate against adults and children living with disability, impairment and ill health and opposes the inclusive mobility of those with disabilities and is unlawful in failing to comply with legal duties under the Equality Act and Public Sector Equality Duties, including the need to make reasonable adjustment and remove or reduce the substantial and disproportionate disadvantage and less favourable treatment disabled people will suffer specifically relating to the discriminating plan to remove the Station Taxi rank from New Station Street.
5. That the guidance for someone who is mobility impaired with a stick, is that they can only reasonably walk for 50 metres, the bus stops are being moved much further than that and that the taxi rank would be positioned an unreasonable and significant distance away from the main station entrance, necessitating the need to find a lift to access the proposed taxi rank on Bishopgate and the scheme does not understand the role taxis and accessible ranks have in enabling and promoting the inclusion and participation of disabled people in society.
6. That the consultation process has not been correctly followed by the Applicant and West Yorkshire Combined Authority (WYCA), prior to submission of the planning application.

7. That there was a delay in the Design and Access Statement being made available to view on the planning application portal on the Council's website.
8. That the new steps appear to be a 'folly' that do not lead anywhere and do not create the statement 'gateway' that they intend. In addition, that the steps would be inappropriate for dealing with large numbers of people with cases and that there may be queues to use the lifts and that the bottom area of the steps and lifts on Bishopgate Street will have too much going on in terms of different uses, to work effectively.
9. That the scheme does not take into account a future Mass Rapid Transit system.
10. That the numbers for the cycle hub are questioned as potentially being too many spaces.
11. That the taxi rank will not be clearly visible to the public when they are leaving the station and this may mean people choose other transport modes (eg private hire) rather than using the taxi rank.
12. What would happen if the passenger lifts were to break down?
13. Some passengers would become reliant upon assistance to guide them to the taxi rank.
14. That it is unfair to allow other vehicles such as emergency vehicles, Police vehicles, servicing vehicles and some train replacement buses to still use New Station Street but to exclude taxis.
15. Has consideration been given to how quickly and efficiently taxis will be able to get through the flow of traffic in the city centre to the rank, given that it only accommodated 6 cars and currently up to 30 taxis can wait on New Station Street?
16. That by having only 6 spaces for taxis at the rank the peak demand will not be met.
17. If the taxi rank was to stay on the New Station Street concourse then the station could be ranked as the fastest moving station in England.
18. That no formal risk assessment for taxi drivers has been undertaken and there is no real justifiable reason why the taxi rank should now be removed and located in what the taxi drivers believe is an incomprehensible position.
19. That passengers will be standing on the pavement at the rank at busy times.
20. That there will be no shelter for the taxi drivers or disabled passengers in inclement weather.
21. That the new arrangement would make social distances impossible.
22. That there is no thought to late night security around the taxi rank.
23. That the scheme would be detrimental on the welfare and income of the taxi drivers.
24. That there has been no consultation with taxi representatives.

Responses:

- Points 1, 3, 4, 5, 8, 10, 11, 12, 14, 15, 16, 17, 18, 19, 20 and 22 will be addressed as part of the appraisal below.

- In respect of Point 2, Princes Square is owned by Network Rail and is the official pick up and drop off area for the station for private vehicles, private hire and also accommodates the short and long stay station car parks. The area is currently being used as the site compound for the platform 0 works. Princes Square is a busy and congested area which in peak times leads to queuing traffic impacting upon the city centre. Princes Square was considered for the relocation of the taxi rank, however, due to insufficient space and the future requirement for redevelopment works at the station with the possibility of a platform -1 and -2 it was considered unsuitable. Network Rail are (separate from this current planning application) proposing to undertake short term works to re-orientate traffic and pedestrian flow in Princes Square which will reduce conflicting moves, improve accessibility and journey times, reduce potential accidents and contribute to improved air quality. It is anticipated that the design works for this will be completed early 2021.

- In respect of Point 6 the Applicant's consultation exercises are detailed in Section 3.2.2 of the Planning Statement and Section 6 of the Design and Access Statement and are in compliance with planning guidance on consultation.
- In respect of Point 7, the Applicant needed to make amendments to the Design and Access Statement which delayed its appearance on the Public Access page of the Council's website. The Planning Application was validated on 7 April 2020 and the Design and Access Statement was uploaded to it on 20 April 2020.
- In respect of Point 9 it is not possible to detail in a designed Mass Rapid Transit system route at this stage due to the fact that the type of system is yet to be settled upon. However the scheme as submitted does protect the route for a future Mass Rapid Transit system to allow it come forward in an integrated manner at a future date.
- In respect of Point 13, Network Rail already provide an optional travel assist service whereby people can be escorted to the existing taxi rank. The Applicant has advised that Network Rail will continue to provide the travel assistance service between the platform and the new taxi rank and will have staff available during the initial weeks following project completion to advise passengers on the changes to New Station Street. In addition hand held tactile maps will be provided on site and at local forums to assist visually impaired passengers along with audio description which will be played through the stations tannoy system providing directions to everyone regarding the new location of the taxi rank and relocated bus stops.
- In respect of Point 21, the Applicant states that should social distancing measures still be in place following the completion of the taxi rank in January 2023, then the appropriate risk assessment will be undertaken and signage installed advising passengers of their responsibility to remain appropriate distances.
- In respect of Point 23, the Applicant has advised that their view is that the increase in user numbers of the station will mean that the taxis serving the area will not see an reduction in income and may see an increase as the number of train passengers increases and that the welfare of taxi drivers to allow them to continue to trade will continue to be a key consideration in the management of the day to day arrangements for the taxis.
- In respect of Point 24, The Applicant has written to taxi representatives at Unite the Union and have advised that they are exploring ways in which to safely host meetings at Council premises whilst following social distancing guidance.

## 7.0 CONSULTATIONS RESPONSES

- 7.1 **Network Rail** state that they are aware of the proposal and are working with the Applicants towards its design and delivery, and that without prejudice to their permitted development rights, which continue to apply on the basis that New Station Street is operational railway land, they have no comment to make at this stage, subject to the continuation of discussions and relevant agreements between themselves and the Applicants in respect of the scheme.  
Response: Such discussions will be continued as required.
- 7.2 **Environment Agency.** No comments have been received.
- 7.3 **Canal & River Trust** state that they welcome the improvements to Dark Neville Street and hope that this will include new wayfinding to Granary Wharf and seek clarification on whether the hard landscaping can be increased to include the access to Dark Neville Street from the canal side area.  
Response; New wayfinding signage will be controlled via a Planning Condition. The area around the canal side access to Dark Neville Street is beyond the scope of the scheme but is likely to form part of future enhancement works under the HS2 proposals.

- 7.4 **Historic England state** that they have no comments to make.
- 7.5 **Yorkshire Water** have stated that they object due to a lack of information in respect to the schemes relationship to a public 450 mm water main that comes close to the proposed steps for the station.  
Response: Further information has been submitted and discussions by the Case Officer with Yorkshire Water highlight that the details required are a technical matter that will be likely to be resolvable by the provision of technical details and by controlling the subsequent agreed approach via Planning Conditions. Officers will update Members verbal at Plans Panel regarding this matter.
- 7.6 **Highways** state that the project is supported in principle and the level of information provided on the highway arrangements at this advanced feasibility stage is sufficient. However, the detailed design is crucial to the scheme's success, as such the a Condition is necessary to agree the detail of the design as it is developed covering the following matters;
- a) The means of control and management of the taxi rank and the feeder rank to it.
  - b) Layout of loading bays.
  - c) Layout and operation of pedestrian crossing points
  - d) Operation of the traffic signals including intelligent technology
  - e) The cycle priority scheme on Dark Neville Street including the junction with Neville Street
  - f) Changes to traffic signs and lining.
- Response: such a Planning Condition will be applied.
- 7.7 **Access Officer** states that they are satisfied that the proposed relocation of the taxi rank has been considered in detail and that the proposed design seeks to deliver accessible solutions by following best practice. That they accept that the rank cannot be retained on New Station Street as it is imperative to pedestrianise this area to aid the dispersion of travellers out of the station entrance area. This change will make the area less congested, noisy and confusing for all users including disabled people. That in their view the design team focused on finding the nearest accessible and safe location for the taxi rank to the main entrance on New Station Street. That the proposed Bishopsgate location offers choices in term of access routes to the taxi rank from the main entrance of the station;
1. Three sets of well-designed steps opposite the main entrance
  2. Two highly visible large capacity lifts also very visible from the main entrance
  3. A ramped route along New Station Street which then brings travellers onto the section of Bishopsgate where the rank is located.
- They stated that they consider it important to clearly delineate this area such that no shared area (where traffic and pedestrians share surfaces which was found to be unsafe and government asked all such schemes be halted in August 2018) or areas which could confuse users are created. The only vehicles which would access New Station Street would be out of hours delivers to businesses and replacement buses loading along the Swinegate section and that introducing a taxi rank onto New station Street closest to the station main entrance would create crowds and queues which would be make this area harder for many disabled people to navigate and would also reduce the speed and efficiency with which travellers can disperse from this area.
- 7.8 **Flood Risk Management** state that a statement of intent submitted to address drainage principles is acceptable but that full drainage details are required under Planning Conditions.  
Response; Such Planning Conditions will be applied.

- 7.9 **West Yorkshire Police** have provide guidance for Secure by Design and for Hostile Vehicle Mitigation.  
Responses; The Applicants have been made aware of this guidance for incorporation in to the detailed design stages of the development.
- 7.10 **Environmental Studies - Transport Strategy** state that they have no objection to this proposal, in terms of its being affected by transportation noise.
- 7.11 **Contaminated Land Team** state that it is recommended in the approved Phase 1 Desk Study report that a site investigation be carried out. As such Conditions should be applied to cover a Site Investigation Report, a Remediation Statement, Verification Reports, imported soils and any removal of asbestos.  
Response; Such Planning Conditions will be applied.
- 7.12 **HS2 Safeguarding.** No comments have been received.
- 7.13 **Public Rights Of Way.** No comments have been received.
- 7.14 **Open Spaces Society.** No comments have been received.
- 7.15 **Nature Team.** No comments have been received.
- 7.16 **Landscape Team** query whether there is the possibility for more street on Bishopgate Street and concourse trees once explorations under existing pavements have been conducted. In addition are there areas of further blank wall to Bishopgate Street that could have green walls?  
Response: The Applicant states the inclusion of street trees has been specifically assessed in respect of the location of below ground utilities, above ground project features and the routes necessary for walking, wheeling, cycling, delivery loading/unloading, passenger waiting space and the other common usages of the urban environment. The study indicated that there were very few locations suitable for street tree locations that did not incur either significant conflict with utility runs, or were poorly placed from a functionality point of view. However, at the detail design stage it will be possible to re-evaluate the location for additional trees and planting.

All landscape matters are to be controlled via Planning Conditions and the addition of any further trees and green walls will be explored at the Condition stage once more survey work has been undertaken.

- 7.17 **Coal Authority** state that the site does not fall within the defined Development High Risk Area and is located instead within the defined Development Low Risk Area. This means that there is no requirement for a Coal Mining Risk Assessment to be submitted.
- 7.18 **Leeds Civic Trust** state that they welcome the work that has been, is being and will be carried out to enhance the travellers' experience of the area and acknowledge the principles behind the scheme currently proposed. They do however have concerns that;
1. There is a lack of wayfinding clarity
  2. The public transport focus appears to be on taxis rather than buses and that bus use is not mentioned as a priority and that the scheme involves the loss of a good bus/train interchange at the station and adds walking distance to relocated bus services at new stops

3. That the bus service relocations should be subject to consultation with users, operators and other stakeholders.
4. That the new steps appear to be the key driver for the project and they cannot see how these will improve pedestrian flows in and out of the station.
5. That pedestrians and cyclists will have to use the same area along Bishopgate Street.
6. That there is also the potential for conflict between pedestrians and cyclists at the junction of Bishopgate Street and Boar Lane where a number of shared-use toucan crossings are proposed.
7. That taxis could use a rank at the Prince Square entrance to the station off Aire Street and what opportunities are there for short stay parking and the long term station plans?
8. That servicing vehicle will require hours of restriction to access New Station Street.
9. That there is an existing issue of bins and beer barrels being stored on New Station Street.
10. The size of the cycle hub and position of cycle routes is questioned.
11. That the taxi rank appears to only provide space for 6 cars and is further away from the station increasing walking distances.
12. Whether or not the scheme has considered the future Mass Rapid Transit?
13. Will the new lifts be able to cope with passenger numbers?

Response: Points 1, 2, 4, 5, 6, 9, 10, 11 and 13 will be addressed in the appraisal below.

In respect of Point 3 regarding the bus service changes consultation the Applicant states that that routing of bus services has been developed alongside WYCA and bus service operators to maximise kerbside opportunities whilst minimising impact on journey times. Traffic levels and the installation of adaptive technologies, the signalised junctions will be designed to limit unnecessary waiting for public service vehicles.

In respect of Point 7 Princes Square is owned by Network Rail and is the official pick up and drop off area for the station for private vehicles, private hire and also accommodates the short and long stay station car parks. The area is currently being used as the site compound for the platform 0 works. Princes Square is a busy and congested area which in peak times leads to queuing traffic impacting upon the city centre. Princes Square was considered for the relocation of the taxi rank, however, due to insufficient space and the future requirement for redevelopment works at the station with the possibility of a platform -1 and -2 it was considered unsuitable. Network Rail are (separate from this current planning application) proposing to undertake short term works to re-orientate traffic and pedestrian flow in Princes Square which will reduce conflicting moves, improve accessibility and journey times, reduce potential accidents and contribute to improved air quality. It is anticipated that the design works for this will be completed early 2021.

In respect of Point 8 an access strategy for service delivery times for the businesses on New Station Street including the station and the Queens Hotel is currently being developed and will be discussed by the Applicant with all relevant stakeholders in due course.

In respect of Point 12 not possible to detail in a designed Mass Rapid Transit system route at this stage due to the fact that the type of system is yet to be settled upon. However the scheme as submitted does protect the route for a future Mass Rapid Transit system to allow it come forward in an integrated manner at a future date.

## **8.0 RELEVANT PLANNING POLICIES:**

- 8.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires the application to be determined in accordance with the development plan unless material considerations indicate otherwise.
- 8.2 The Development Plan for Leeds currently comprises the following documents:
11. Core Strategy 2014 (as amended by the Core Strategy Selective Review 2019)
  2. Saved Leeds Unitary Development Plan Policies (Reviewed 2006), included as Appendix 1 of the Core Strategy
  3. The Natural Resources & Waste Local Plan (NRWLP, Adopted January 2013) including revised policies Minerals 13 and 14 (Adopted September 2015).
  4. Leeds Site Allocation Plan 2019
  5. Aire Valley Leeds Area Action Plan (Adopted November 2017)
  6. Any Neighbourhood Plan, once made.
  7. Site Allocations Plan ('SAP') (adopted July 2019)
- 8.3 **Core Strategy**
- 8.4 The Core Strategy sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. Relevant Core Strategy policies are as outlined below.
- 8.5 **Spatial Policy 3: Role of Leeds City Centre**  
Seeks to maintain and enhance the role of the City Centre as an economic driver for the District and City Region. This includes comprehensively planning the redevelopment and re-use of vacant and under-used sites for mixed use development and areas of public space; enhancing streets and creating a network of open and green spaces to make the City Centre more attractive; and improving connections between the City Centre and adjoining neighbourhoods
- 8.6 **Spatial Policy 11: Transport Infrastructure Investment**  
Sets out a series of spatial priorities for the delivery of an integrated transport strategy for Leeds. One priority is related to improved facilities for pedestrians to promote safety and accessibility, particularly connectivity between the edges of the City Centre and the City Centre
- 8.7 **Policy P10: Design** states that:  
New development for buildings and spaces, and alterations to existing, should be based on a thorough contextual analysis to provide good design appropriate to its scale and function.  
New development will be expected to deliver high quality innovative design that has evolved, where appropriate, through community consultation and which respects and enhances the variety of existing landscapes, streets, spaces and buildings according to the particular local distinctiveness and wider setting of the place, contributing positively towards place making and quality of life and be accessible to all.
- 8.8 **Policy P11: Conservation** states that development proposals will be expected to demonstrate a full understanding of historic assets affected. Heritage statements assessing the significance of assets, the impact of proposals and mitigation measures will be required to be submitted by developers to accompany development proposals.
- 8.9 **Policy T1: Transport Management** states that support will be given to the following management priorities:

c) To support wider transport strategy objectives for sustainable travel and to minimise congestion during peak periods.

8.10 Policy T2: Accessibility Requirements and New Development states that new development should be located in accessible locations that are adequately served by existing or programmed highways, by public transport and with safe and secure access for pedestrians, cyclists and people with impaired mobility:

### 8.11 **National Planning Policy Framework (NPPF)**

8.12 The National Planning Policy Framework (NPPF) was revised and adopted in February 2019 and sets out the Government's planning policies for England and how these should be applied.

8.13 Paragraph 110 states that priority should be given to pedestrian and cycle movements; the needs of people with disabilities and reduced mobility addressed; creation of safe, secure and attractive spaces; allow for the efficient delivery of goods; and be designed to enable use by sustainable vehicles.

8.14 Paragraph 127 states that decisions should ensure that developments:

- a) Will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) Are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- c) Are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) Establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) Optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) Create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

8.15 Paragraph 195 of the NPPF states that where a proposed development will lead to substantial harm to (or total loss of significance of) a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or total loss is necessary to achieve substantial public benefits that outweigh that harm or loss

8.16 Paragraph 196 of the NPPF states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

### 8.17 **The Site Allocations Plan (Adopted July 2019)**

8.18 The Site Allocations Plan (SAP) states that Leeds City Centre is the main retail and service centre for the city. The site has no specific allocations or designation under the SAP although the nearby City Square is designated as a Green Space.

## 8.19 **Supplementary Planning Guidance**

Street Design Guide (August 2009)

Building for Tomorrow Today: Sustainable Design and Construction (August 2011)

Accessible Leeds (adopted November 2016)

## 9.0 **MAIN ISSUES**

1. Principle of the proposed use
2. Climate Emergency and Sustainability
3. Sustainable transport
4. Accessibility
5. The impact of the proposed design on the character and visual amenity of the host site and surrounding
6. Landscaping and public realm

## 10.0 **APPRAISAL**

### 10.1 Principle of the Use

10.2 Leeds City Station is one of the busiest stations in the North of England with passenger numbers set to increase over the next 20 years. The proposals aim to assist the station to manage this increase in passenger numbers, by creating an enhanced, pedestrian focused, accessible arrival space that allows the safe management of people and addresses counter-terrorism concerns by removing general traffic from the area. This will also result in the relocation of the existing taxi and bus services, the reconsideration of servicing requirements and cycle parking and route connections improvements. These changes, combined with a resurfacing of the area to the front of the train station, with opportunities for new high quality hard and soft landscaping, would allow for the creation of a new user friendly, pedestrian focused space.

10.3 The scheme is also part of a wider programme of transformational transport initiatives to be delivered across Leeds, which aims to reduce vehicle usage within the city centre. The works are designed to encourage a modal shift towards sustainable forms of transport by creating user friendly spaces that support opportunities for cycling, walking and connectivity to other forms of transport. The scheme will also support the City Council's Climate Emergency agenda (expanded below).

10.4 The use of the new kiosk is proposed to be as an A3 café/coffee shop. . The proposed use would be an appropriate city centre use that would add to the vibrancy and vitality of the station area and the wider Leeds City Centre.

10.5 The proposals therefore accord with local and national planning policies and in particular with the aims and objectives of policies Spatial Policy 3, Spatial Policy 11, P10, T1 and T2 of the Core Strategy and paragraphs 110, and 127 of the National Planning Policy Framework.

### 10.6 Climate Emergency and Sustainability

10.7 Members will be aware that the Council has recently declared a Climate Emergency. Existing planning policies seek to address the issue of climate change by ensuring that development proposals incorporate measures to reduce the impact of non-renewable resources.

10.8 Sustainable transport and the improvement of the environment and air quality in the immediate area are key drivers of the proposal, with its aims to reduce vehicle movements (and associated emissions), to promote walking and cycling in the area and to create a more pedestrian focused and accessible entrance to the train station, there are also proposals to green the area, with different types of planting including trees. Biodiversity enhancements will also result via the inclusion of green roofs to the taxi shelter and kiosk, and soft landscaping in the planters and terraces, as well as a green wall to the lift tower. In addition the creation of new greened public space will enhance well-being for users of this area.

10.9 Sustainable transport

10.10 The proposal fundamentally seeks to promote sustainable forms of transport and to do so a number of changes are required to the existing transport network in the immediate area. These are being designed to connect to and co-ordinate with the Council's broader programme of transformational transport projects. The proposed changes for this immediate scheme currently presented to City Plans Panel are as follows:

- A reduction of the width of Bishopgate Street to one lane, to allow for wider pedestrian footways, new segregated cycle lane provision and the relocation of the existing station taxi rank to a newly laid out area on Bishopgate Street.
- The new sweeping steps and passenger lifts will connect to Bishopgate Street from the new station arrival space.
- A reduction of the existing carriageway under the Neville Street bridge to one lane each way, to create wider pedestrian footpaths and segregated cycle lane route connections.
- New Station Street will be pedestrianised with only access allowed for emergency, servicing and British Transport Police and for replacement bus services for train cancellation vehicles to create a traffic calmed, pedestrian focused area. The removal of general traffic will also assist to address concerns regarding potential vehicle incursion of the station either by accident or by attack.
- The existing bus service routes that currently use the train station interchange will be relocated so that they avoid this arrival area that is to be pedestrianised. This means that the buses in question will be rerouted to run along Boar Lane, Mill Hill and along the eastern side of City Square to connect to Infirmary Street (which will become a two way street under a separate highways scheme).
- A new 700 space cycle hub will be created which would be accessed via an entrance close to the northern end of the Neville Street Bridge. Facilities will include space for electric bikes and maintenance and repair services.

10.11 The proposals include the moving of the existing taxi rank to a new purpose built stand on Bishopgate Street. Objections received state that the rank should be retained in its current position to allow the station to become the fastest moving station in England. The Applicant states that this is not possible due to the need to traffic calm the area along New Station Street to allow this concourse area to be able to meet a predicted 81% increase in user numbers by 2043, to ensure the future operation of the station does not diminish. It is the case that other options for the location of the taxi rank were considered during the initial project scoping, included directly outside the main station entrance, adjacent to the vehicle mitigation bollards, by the Platform building and Princes Square. However assessments of these locations found them to be unsuitable for the taxi rank as a result of the aforementioned increase in pedestrian capacity on New Station Street. It is also the case that the current rank operates with the following operational and accessibility issues;

- To reach the current taxi rank from the station there is a need to cross vehicular lanes which carry taxis, buses, emergency vehicles and delivery vehicles.
- The current route, although recently improved, still requires people to navigate a narrow footway which runs along the frontage of the existing Cycle Point building, with vertical street furniture located in the pedestrian route, further narrowing the available space.
- To get to the existing taxi rank from Bishopgate Street, users must navigate to either end of New Station Street (City Square or Boar Lane ends), or use the partially enclosed existing staircase.
- The existing rank's loading is from the driver's side of the taxi. Wheelchair accessible taxis are generally designed to be loaded from the passenger's side or the rear. As a result, the taxi rank has a specific crossing point to an island to enable boarding of wheelchair users from the right or the rear, however this is not at the front of the queue.

- 10.12 Concerns have also been raised by the Objectors about the removal of taxis from New Station Street when other vehicles will still be able to use the street. As stated above, the increase in passenger numbers arriving and exiting the train station is a major factor influencing the approach and layout of the scheme. The Applicant states that by 2026 passenger numbers (predicted to be in excess 45 million by 2023 and 70 million by 2043) will be at a level where, if no action is taken, the station's gateline and boarding procedures will need to be managed. Similar to London Kings Cross Station this will mean that passengers will be held back in the southern concourse and only called forward to the platform once the train is ready to depart. The southern concourse is limited in space and given the number of platforms there are at Leeds Station, this will result in significant numbers of people, particularly at peak times, spilling into New Station Street increasing the risk of pedestrian / vehicle conflict on New Station Street. As the majority of these passengers will arrive and leave the station on foot, this increases space needed to safely accommodate these additional users. Therefore, a key aim of the proposals is to reduce the impact of vehicles on the station approach. This means that along New Station Street the majority of vehicle movements will be removed. Due to specific needs, provision will still be required for emergency vehicle, replacement bus services for train cancellations, British Transport Police vehicles and the servicing of businesses operating along New Station Street. However, to achieve the space requirements to accommodate the predicted passenger numbers New Station Street need to be traffic calmed as much as is possible and this is why taxis, general buses and general traffic would no longer be able to use the space. Servicing arrangements will be time limited and designed to avoid peak pedestrian flow hours.
- 10.13 Therefore the new taxi rank is to be positioned on Bishopgate Street. The current distance from the platform gateline within the station to the existing taxi rank is some 115 metres. The proposed taxi rank on Bishopgate Street would be some 130 metres distance from the said gateline.
- 10.14 Whilst this does mean that the taxi rank is further away from the station entrance, by an additional 15 metres from the position of the existing rank, there are a number of improvements that result from this relocation as follows;
- The new taxi rank will be a large purpose built shelter that would be a 21.6 m long (22.8 m long roof), 3.2 m wide enclosure (5.3 m wide roof), and 3.2 m in height to the top of eaves line.
  - The existing taxi rank accommodates 3 vehicles with a feeder rank set out for 25 vehicles along the length of New Station Street (although comments from Objectors state that up to 30 cars can park up on New Station Street at any one time). By

contrast the new taxi rank on Bishopgate Street can accommodate up to 6 taxis and is to have a series of successive feeder ranks, with space to be set out on Swinegate to accommodate 14 vehicles directly underneath the bridge and the option to utilise Concordia Street and/or Sovereign Street, with space for 12 vehicles (giving a total of 32 spaces for taxis in combination). As such, together, the new rank and these feeder ranks will provide more capacity for waiting vehicles than the current rank on New Station Street. Taxis feeding the rank on Bishopgate Street will wait in their dedicated lane on Swinegate and will be called forward to the taxi rank under traffic signal control at the junction of Mill Hill/Swine gate/Bishopgate. The timing of the traffic signals will enable up to 6 taxis to be called forward to the rank. To supplement this the intention is to retain the Taxi Marshall (which will also assist to address security concerns) and relocate them to Bishopgate. The Applicants have calculated that by using this set up this will allow for between 229 to 284 taxis per hour being able to access the rank via the feeders, depending on use of the new toucan crossing. This equates to 297-369 passengers per hour travelling to suburban areas and 343-426 inner-city passengers per hour.

- The proposed rank is designed to load from the passenger and rear side of the taxi and as such has multiple points of entry to allow for such loading. This means less pressure for people who require assistance to access the taxi, as vehicles behind the front vehicle can load and depart without having to wait for the assistance-required taxi to depart first.

- A direct route from the station entrance to the rank on Bishopgate Street is designed in to the scheme. This would follow a straight-line route, demarcated in a contrasting colour from the surrounding surfaces. Concerns have been raised by Objectors that the rank will not be clearly visible to the public when they exit the station. Clear directional signage and wayfinding will be installed on New Station Street, Bishopgate Street and within the northern and southern concourse areas of the station. Signage and wayfinding will be designed in consultation with the Access and Usability Group, the appointed inclusive design specialist and the Council's Economic Development Team who are responsible for wayfinding across the city centre.

- Access between the different levels of New Station Street and Bishopgate Street and the taxi rank is proposed to meet a range of needs. Two large passenger lifts are proposed. These are strategically located at a prominent location in front of the station entrance. The lifts are large, being 21-person capacity, and are appropriate for large volumes of people, and those with luggage / wheelchair / mobility scooter / young children / pushchairs etc. Objectors have questioned what would happen if a lift broke down. By using two lifts, additional capacity is enabled and allows for the continued functioning of one lift should maintenance or closure of the other lift be necessary. The Applicant advises that Network Rail have a standard 2 hour response time for the contractor to attend site, the rectification time will then depend on the type of fault found, that if a person is trapped within the lift then Network Rail will endeavour to rectify this within 30 mins and that daily checks of the lifts will also take place by the station team. In the unlikely event that both lifts fail at the same time then step free access to the taxi rank will be provided by travelling along New Station Street, turning right onto Bishopgate at the Queens Hotel (a distance of some 160 metres).

- The lifts are intended to be partially transparent, with glass to some elevations and with a modern and clean aesthetic. In addition, the lift entrances at both New Station Street and Bishopgate Street levels have been carefully located directly adjacent to the new commercial space to offer natural surveillance.

- The lift core location also forms the edge of the ambulant step route from the station entrance to the taxi rank. This allows for a physically more legible route for visually impaired people who wish to use steps to 'follow the building line' in order to navigate to the steps and rank.

- Alternatively, multiple stair options are provided. Commentators have raised a question about whether or not there are too many sets of steps. As already stated in this report, it is the case that passenger numbers are increasing. Therefore, providing substantial areas of steps alongside lifts is required to allow the projected increases to be catered for. As such, the stairs are proposed in a broad sweep, to accommodate large numbers of people at busy periods.
- One of the stair flights accommodates a bike ramp which allows cyclists to push their bike up or down the stair flight. Its location has been selected to reduce conflict with users of the stairs who may require assistance / handrails / the most direct ambulant route between the station entrance and taxi rank.
- The proposed taxi rank shelter itself includes seating for 15 people), together with seating in the public realm at New Station Street level. Although the route between rank and station is further, the seating and canopies in the shelter and at the lifts offers mid-points for people to rest or break the route into smaller sections.

- 10.15 Concerns have been raised by objectors with regards to the removal of the bus stops on New Station Street and Bishopgate Street and the moving of bus services to other stops outside the station, in respect of increased walking distances to these stops and the loss of the train –bus interchange arrangement that is currently on New Station Street. The Applicants have advised that in order to accommodate the projected increase in station user numbers, and to create a safe environment for these pedestrians accessing the station, New Station Street must be trafficked calmed by the relocation of bus services. The exact locations for the re-sited services are still the subject of ongoing discussions by the Applicants with bus operators and all other key stakeholders. However, to minimise walking distances as much as is possible, the closest stops to the station will be utilised as well as there being potential for new stops to be created on Boar Lane. The existing distances from the platform gateline to the bus stops that are to be removed ranges from 60 metres to 160 metres. The new locations for the repositioned bus services would range from 235 metres to 375 metres from the said gateline depending on the services and which stops can accommodate the relocations. Therefore, for example, it is likely that the airport bus and some rail replacement buses would be moved to the first stop on Wellington Street opposite the Majestic building, allowing them to be as close to the train station’s north-west and west entrance/exits as possible. Bus stops on Aire Street, Wellington Street, Boar Lane and Infirmary Street would be likely to accommodate the other relocated services. The proposed rail replacement bus services will be located at stops between 100 metres and 255 metres from the platform gateline.
- 10.16 The existing carriageway, covered by the scheme, will be replaced with improved pedestrian links and crossings at multiple locations. Routes to loading bays and zones will be retained and will become time-restricted. Objections received have questioned the value of the new steps linking the station to Bishopgate stating that most users are likely to exit towards the city. Whilst it is true that a high number of uses will head in the direction of the city centre, the continued expansion of the city centre in to the Southbank area means that future footfall is predicted to increase significantly to and from the south of the city centre. Therefore, the proposed steps and passenger lifts will help to accommodate this change in passenger numbers from this side of the city centre and will offer users more options for their direction of travel.
- 10.17 Pedestrian crossings will be reconfigured to address the new focus on pedestrian movements with defined crossing in the following locations;
- Across Swinegate approaches at Mill Hill junction;
  - Across Mill Hill approach at Swinegate junction;

- Across right-turn lane into Neville Street;
- Across Neville Street northbound lane close to junction with Bishopgate Street;
- Toucan crossing on Bishopgate Street, north off Neville Street entrance;
- Across Boar Lane, east of Bishopgate Street;
- Across Bishopgate Street, south of Boar Lane;
- Across New Station Street, in line with Bishopgate Street pedestrian crossing south of Boar Lane.

Leeds Civic Trust have raised concerns that there is the potential for conflict between cyclists and pedestrians at shared-use toucan crossings. The Applicant has advised that the width and provision of crossings will be reviewed and adjusted during the detail design stage in conjunction with the signal timings and highways layouts to enable all modes, including cycling and walking, to operate effectively. The number of crossings is considered to be appropriate to allow for junctions to operate and their locations have been selected to strategically link the primary crossing points for both pedestrians and cyclists.

- 10.18 The new cycle hub would provide secure high quality parking for some 700 bicycles in converted arches and the former mill goit under New Station Street, accessed from a new entrance on Bishopgate Street. Comments received question why this number of spaces has been proposed. The Applicant has advised that this number is proposed to allow the scheme to effectively improve on the existing provision and to respond to the Leeds Public Transport Investment Programme (LPTIP) cycling promotion aspirations via the provision of cycle facilities that can meet potential future demand. As such the proposed cycle hub is envisaged to serve both the station and the wider city centre and to expand the provision of cycle parking over that currently provided. As such the layout of the cycle hub, across different underground spaces means that the provision could be brought forward in a phase manner in response to need.
- 10.19 In addition, the cycling in Leeds City Centre will be further supported by the installation of new high quality, clearly-defined and segregated cycle lanes on routes along Dark Neville Street, Neville Street and Bishopgate Street. Concerns have been raised by Objectors regarding the potential for conflict between pedestrians and cyclists on Bishopgate Street and whether the cycle lanes could be on the opposite side of the road. The layout for the cycle route, street and taxi rank has been carefully considered to minimise the number of potential pedestrian, cycle and vehicle conflicts at this busy location. As a result the area will be laid out to direct all cyclists to use the segregated defined two way cycle route. The proposals will ensure safer and a more continuous cycle network connecting the station with local cycle routes and key destinations. To accommodate a two way segregated cycle route under the Neville Street bridge, that will connect into a two way cycle route on Bishopgate Street, the northbound carriageway under the bridge will become bus only and the southbound carriageway will accommodate buses, taxis and general traffic. Dark Neville Street will also include a segregated cycleway, with a raised pedestrian crossing point to allow access to and from Little Neville Street.
- 10.20 These works will be subject to detailed designs for these elements and co-ordination with all key stakeholders to ensure that bus services, taxi provision, cycling and walking routes are set out and routed logically with the minimum of disruption and that service levels are retained for users of public transport. However, Officers consider that the principles of these moves towards more sustainable transport in this area accord with Core Strategy policies Spatial Policy 3, Spatial Policy 11, T1 and T2 of the Core Strategy and paragraphs 110 and 127 of the National Planning Policy Framework.

10.21 Accessibility and Inclusivity

10.22 Local Planning Policy seeks developments to adopt a 'pan-disability'/ 'pan-impairment' approach, taking into account and creating an inclusive environment for people with a wide range of impairments. Good building and spaces design should ensure accessibility for all.

10.23 Objectors have raised concerns regarding the additional distances that users need to travel from the station entrance to the new taxi rank and the relocated bus services at new and existing bus stops, stating that this would exclude and discriminate against adults and children living with disability, impairment and ill health and opposes the inclusive mobility of those with disabilities and is unlawful in failing to comply with legal duties under the Equality Act and Public Sector Equality Duties. As stated above in this report, the fundamental requirement for the scheme is to create an environment which can safely and effectively cope with the significant increase in passenger numbers that are predicted to need to use the train station. This means that the spaces around the station entrance and the taxi waiting provision need to be enhanced to create a safer, more fit for purpose station entrance. It is the case that to allow the improvements to come forward to meet this predicted demand, the taxi rank and bus stops do need to be relocated. This does mean that the taxi rank and bus stops will be further away from the station entrance, but does provide the opportunity to create a larger, purpose built taxi rank that can accommodate more taxis and more passengers to reflect the increase in user numbers.

10.24 The scheme has therefore been developed to take into account the following inclusive design good practice guidance:

- BS8300:2018, Design of an accessible and inclusive built environment – Part 1 External Environment – Code of practice.
- BS8300:2018, Design of an accessible and inclusive built environment – Part 2 Buildings – Code of practice.

As such the scheme includes the following inclusive design principles.

10.25 The New Taxi Rank

- Bespoke purpose built taxi shelter designed to provide kerbside space for the deployment of ramps from the side and rear of taxis.
- As detailed above in paragraph 10.14, the new taxi rank will be able to park up and load passengers in to 6 cars at any one time, with other taxis being able to wait close by in a feeder queue for up to 14 vehicles on Swinegate and with the option to use Concordia Street and/or Sovereign Street for a further 12 vehicles, meaning that no passengers should be disadvantaged in respect of waiting times.
- The new taxi rank will allow embarkation of vehicles on the correct side of the taxis (the current rotunda taxi rank does not allow for this and some users have to approach taxis from the wrong side).
- The taxi shelter will be of a length to allow a number of taxis to wait at any one time, with all access points being designed to take account of the need for taxi ramps to be deployed.
- The taxi shelter to have both internal seated (of differing heights and fitted with arm and back rests) and standing areas and is to be appropriately light inside and outside.
- The taxi shelter will be fitted with an overhanging canopy at the rear, providing protection to passengers and to taxi drivers when they are out of their cars assisting passengers, in inclement weather.
- The taxi rank is to have clear signage and glazing is to be treated with contrasted manifestations (BS 8300).

- Although there is an increase of 15 metres in the distance to the taxi rank from the train station platform gateline (giving a full distance of 130 metres), the scheme is favourable in comparison with other train stations in other large city centres (eg the distance between the station gate and the taxi rank at Birmingham New St is some 183 metres).

#### 10.26 Bus Stops Relocations

- Whilst distances between bus stops and the platform gateline will increase as a result of moving the buses off New Station Street, all bus services will be located to the closest bus stop possible outside of the train station as discussed above in paragraph 10.15.
- Flat and level pedestrian routes along New Station Street will allow users to progress to nearby bus stops

#### 10.27 The New Steps

- The new steps will be constructed with risers and goings of dimensions to meet best practice and flights will be aligned to building lines to aid detection for people with visual impairment.
- Landings are to be provide as rest points at regular intervals on the stairs.
- Tactile paving is to be installed to the tops and bottoms of all steps.
- Steps are to be treated with contrasting nose edgings.
- Left and right handrails are to be provided for the steps and to incorporate downlighters to illuminate the steps.
- The steps would have generous widths to reduce perceptions of confinement in small spaces (this is a significant upgrade over the current covered stair rotunda).
- The stairs are arranged in multiple distinct flights. This allows each separate flight to maximise accessibility by following a straight-line geometry. None of the stairs are curved or tapered, and the user is able to navigate the stairs and handrails at 90 degree angle from the adjacent footways, minimising the risk of overstepping or miscalculating the direction of stair flights

#### 10.28 The New Lifts

- The two new lifts are to be designed to meet the safety and accessibility requirements of BS8300-2:2018 and BS EN 81-70:2018.
- The two lifts will be large enough to accommodate 21 people in each lift.
- Call and control buttons are to have tactile indicators.
- Visual and audible indicators will also be installed to advise users that lifts are moving and/or have arrived. Audible announcements will also advice users when the lift doors are opening or closing.
- Entrance doors to the lifts will be slow opening, and will include glazing to allow users to see into and out of the lifts. The sides of the lift will be solid to allow for the location of the lift control panels.
- Each lift will be fitted with internal handrails.
- Canopies will be provided at the lift entrances to provide shelter whilst users are waiting.

#### 10.29 The Reduction in Vehicles on New Station Street and the Relaying of the Spaces

- The removal and reduction of the different forms of vehicles that currently use New Station Street will allow the spaces along it to become more pedestrian focused, allowing for the creation of larger, safer space for all people, including those with reduced risk perception.
- The reduction in vehicle traffic will mean an associated reduction in noise levels, of potential benefit to a variety of neurodivergent and hearing impaired users.
- The reconfigured spaces are to have a simplified layout to delineate movement, direction and uses,

- Outside of the enlarged pedestrian priority area, kerbs will delineate the route for cyclists and occasional vehicles and will provide a detectable edge for blind and partially sighted users.
- Routes for cyclists will be set out to achieve the shortest distance to the cycle hub and to avoid crossing the pedestrian flows around the bottom of the lift and steps.
- A strong defined pedestrian route will be created between the station entrance and the taxi rank via the positioning of street furniture, the use of demarcation and lighting, the layout, type and colour contrast of paving for easier detection (both visually and physically).
- Key pedestrian routes and areas will be defined as 'cyclists dismount' areas to ensure pedestrians have priority.
- A variety of seating types will be provided at regular intervals, including accessible and varying height seating with all seating to meet the requirements of BS 8300. Seating will also be integrated into the landscaped public realm features.
- Appropriate lighting, designed to meet best practice to serve both wayfinding and safety, will be installed in all areas of the newly reconfigured concourse and routes, to ensure a safe and inviting environment is created for users.

10.30 Objectors have also raised concerns that there will be a lack of clarity for users as to which route they should take when exiting the station. Clear directional signage and wayfinding will also be installed on New Station Street, Bishopgate Street and within the northern and southern concourse areas of the station as part of this project. The signage and wayfinding will be designed by the Applicant in consultation with the Access and Usability Group and the appointed inclusive design specialist and the Council's Economic Development Team who are responsible for wayfinding across the city centre.

10.31 As such whilst it is acknowledged that there is an unavoidable increase in distances to the taxi rank and the bus stops, it is in that case that the scheme provides the opportunity to increase pedestrian priority in the area, to create an inclusive taxi rank designed to allow easy and safe orientated embarkation and to create spaces and routes which acknowledge accessibility needs, whilst addressing the predicted future increase in the numbers of users of the station, which will result in an associated increase in use of the new taxi rank and the nearby bus services.

10.32 It is therefore considered that the scheme embeds accessibility into layout and design and through its external spaces. More specifics on the elements of the accessibility features of the scheme will come through in the detailed stages under Planning Conditions submissions should Members be minded to support this Planning Application to ensure that the scheme does come forward in an accessible and inclusive manner as is possible.

10.33 The impact of the proposed design on the character and visual amenity of the host site and surrounding

10.34 The proposal would involve a number of physical alterations to the area between the train station and Bishopgate Street to facilitate the creation of the new pedestrian focused arrival space. Key to this is the removal of the existing supporting wall that edges the current bus interchange in front of the train station and fronts on to Bishopgate Street. This supporting wall addresses a levels change between the interchange and station area and Bishopgate Street. Therefore, it is proposed to replace this supporting wall with new structural walling fronted by a series of grand sweeping steps, in appropriate high quality materials, a new kiosk and pedestrian lift building and a new entrance to the new cycle hub space. A new free standing taxi

shelter would also be sited on Bishopgate Street close to base of the new steps and lifts.

- 10.35 The wall to be removed is not itself a listed structure but is attached to the Grade II listed Queens Hotel, as well as to the Grade II listed vaulting and basements beneath the hotel and the station Concourse structures. As a result an application for listed building consent has been submitted for this element of the entrance works. The section of the wall to be removed is utilitarian in appearance and is unremarkable in respect of its historical and architectural merits. The removal of the wall and its replacement with the new steps, kiosks and lift building will not intrude on the fabric of the hotel, due to the new flank wall facing on the stairs being structurally independent of the hotel's hidden south east wall. The utilitarian, unremarkable character of the wall would mean its removal would have only a minor adverse impact on the hotel's significance.
- 10.36 As noted in paragraph 3.2 above, the site is also within setting of a number of other Grade II and Grade II\* listed buildings and the City Centre Conservation Area. The proposed new steps and kiosk building are designed to be respectful, appropriate additions in respect of their character and impact on this heritage rich setting. As such the aim is to introduce sensitively detailed complimentary, rather than competing, structures that enhance the setting of the nearby listed buildings and the City Centre Conservation Area.
- 10.37 The new kiosk building would be predominantly a two storey structure, the height of which is largely determined by the manner in which it traverses the levels change between Bishopgate and the station concourse at New Station Street. The scale ensures that the building would be subordinate to other existing buildings around it in the street scene. However, a point of height will be created in the structure due to the placement of the new pedestrian lifts in a small tower. This tower is also intended to be a way-finding device that would be visible from within the upgraded public realm and from the train station exit. To this end its verticality is to be emphasised by a lantern feature to its top, which would be capable of conveying electronic information to the public such as the time and the temperature. The building is designed to be a contemporary addition to the streetscene. The form of the building would mean it would have a simple but bold horizontal roof form, edged in bronze tone metal cladding, with a feature green roof upon it. This level roof form would accentuate the lift tower, and would terminate in a canopy overhanging the building beneath it. The facades of the kiosk and lift tower would be planar frameless glazing (with decals to aid accessibility) to the Bishopgate and station faces, to allow views in and out of the structures, with surrounds of Yorkstone and bronze toned metal cladding to both elements. The two sides of the kiosk and lift tower would flank the new steps and would have different facades. To the north facing side of the structure the façade would be dressed in a green wall. To the south facing side the façade would be a more opaque channel glazing system (e.g. a Reglit type of system).
- 10.38 To create cohesion the proposed cycle hub entrance and the new standalone taxi shelter will also be designed with the same aesthetic approach as the new kiosk and lift tower. As such these too will be structures with large areas of glazing (treated with the appropriate decals to aid accessibility) with bronze clad framing. All structures will take account of counter-terrorism requirements in respect of their design to ensure effective blast mitigation measures are incorporated. The new taxi rank shelter is designed to be complimentary but visible, being of some 21.6 m in length by, 3.2 m in width and 3.2 m in height to the top of eaves line. As such it should be readily viewable for people approaching the station from Park Row, City

Square, Swinegate, Mill Hill and Neville Street. Inside the cycle hub areas of cycle parking will be laid in the arches and the former mill goit, beyond an access secured entrance lobby and a unit for a potential cycle related repair-shop and toilets, shower and changing areas. The existing arches to Sandford Street, accessed under the Neville Street Bridge will be re-elevated with glazing in the archways, allowing views in and out of the cycle hub.

- 10.39 The current environment under Neville Street Bridge is dark and oppressive, with vehicular traffic being a dominant feature causing dirt, air quality and noise issues. The proposals seek to make significant improvements to this with works to encourage a reduction in the volumes of traffic, the relaying of the public highway to create narrower carriageways. This will allow footways to be widened and for the creation of a new segregated two way cycle route. New lighting is proposed and will be designed to prevent the 'black box effect' traffic currently suffer when approaching the bridge. The new lighting would also effectively and appropriately illuminate the pedestrian footways, creating a safer feeling to the route. The current dark 'feature' cladding to the walls (and soffit over the footways) will be removed and will be replaced with light coloured demountable perforated panels (with an acoustic absorbent layer behind), decorated with art and/or heritage images, with the new lighting above the footways set in to new soffits of feature fabric metal mesh.
- 10.40 Dark Neville Street is quieter in respect of the level of traffic and has had some upgrade work undertaken to its furthest end, with paving beyond the bridge, a new entrance to the train station and feature, colour change lighting that accentuates the vaulted roof form. It is proposed to add to these past works by relaying the surfacing of the street, creating defined pedestrian and cycling routes. New downlighters will replace and upgrade existing downlighters that sit beneath the existing roof feature lighting, to illuminate the new foot and cycle ways. In addition, at present a large number of the arches off Dark Neville Street are either vacant or in some form of functional/storage use. It is hoped that at a future date more active uses will be found for these arches. In the meantime these will be screened by way of temporary architectural frontages that will illustrate art and/or heritage imagery.
- 10.41 On the basis of the information provided thus far Officers consider that the level of demolitions and alterations will be likely to lead to less than substantial harm to the significance of these designated heritage assets and the character of the City Centre Conservation Area, and as such the proposals can be justified in accordance with the considerations of paragraph 196 rather than paragraph 195 of the NPPF. Case law has held that once harm to a heritage asset is established, considerable weight needs to be attached to that harm.
- 10.42 The public benefits to the scheme include:
1. The provision of a state of the art multi-modal transport interchange.
  2. The promotion of sustainable modes of transport.
  3. Improved pedestrian routes along New Station Street from the junction with Boar Lane to the point where it meets City Square and connecting the station to Bishopgate and route to the south and east of the city centre.
  4. An improved environment with reduced traffic levels in the area
  5. An enhanced entrance for the station and defined gateway to the city.
  6. Accessible connections to the station from Bishopgate via the new pedestrian lifts.
  7. The creation of a new purpose built taxi shelter, with a larger waiting area than the current provision, better access arrangements for getting in and out of waiting taxis and covered seating.
  8. Improved, defined and connected cycle routes.

9. Improved and expanded cycle parking provision, encouraging the use of this sustainable mode of transport.
10. Green landscape features including trees to create an attractive, environmentally positive area outside the station entrance.
11. New level high quality paving, creating level accessible attractive streets and spaces.
12. Environmental enhancements to the area under the Neville Street Bridge and along Dark Neville Street to make these places feel safer and more attractive to users.

10.43 Therefore, whilst it is acknowledge that there is harm in the loss of the historic wall this is considered to be less than substantial. It is also considered that the harm is outweighed by the benefits of providing this high quality, appropriately designed transformational transport initiative.

#### 10.44 Landscaping and public realm

10.45 The Applicant has stated that they understand the need to include green infrastructure in the project and that the benefits brought, both in terms of performance and visually from green elements, being a strong part of the urban environment, are considerable. As a result the principle aim of the new public realm is to create a pedestrian focused area that can adapt to increasing numbers of train passengers. Therefore, more generous and better accessible public routes to and from the station concourse are required. The landscaping aims to create legible dispersal and escape routes should an incident occur at the station, as well as making safe and welcoming, connected spaces and routes that allow the free flow of pedestrians through them.

10.46 To achieve the above aspirations the proposal is to relay the new entrance area, and the route along New Station Street, with high quality pavers and setts to all pedestrian zones, with full height panels and support post to areas needing balustrades. To this end, a high quality palette of materials is proposed, including Yorkstone paving, hardwood timber seating and feature stainless steel, granite and concrete steps. Some seating will be integrated into the landscape structures and a variety of types and heights of seating will be provided. Where existing refuse and recycling bins and beer barrels are being stored on New Station Street (for businesses who have no back of house areas to accommodate these) these will be concealed behind architectural screening, of a design to be agreed under a Planning Condition. Where it is necessary to have vehicle trafficked areas on Bishopgate Street, the aim is to lay these with asphalt concrete surfacing. Automated hydraulic hostile vehicle mitigation bollards are to be positioned to the areas where vehicle admittance will be restricted.

10.47 To green the new public realm area and routes along New Station Street soft landscaping will be provided in raised planted beds, with integrated seating, to the concourse and between the steps where there are opportunities for tree planting as well as ornamental shrubs, grasses and meadow planting. Some of these will be ground planted in urban tree pits with cellular support systems, whilst other planting will be in raised planters due to the voids that are under New Station Street (I.e. the mill goit, arches and back of house tunnels that serve the Queens Hotel). Green roof structures are proposed to the cycle hub and taxi shelter, and green wall treatments will also feature on the lift tower building. Further opportunities for any additional tree planting or green wall installation will be explored at the detailed design stages, once physical surveys of what is under the streets in question have been actioned. As such full details of the landscape scheme will come forward under relevant

Planning Conditions to ensure all potential locations for planting are given consideration.

## **11.0 CONCLUSION**

- 11.1 In conclusion it is considered that the proposal would result in a high quality, appropriate development, that would allow Leeds Train Station to adapt to a predicted, significant increase in user need, whilst create a traffic calmed enhanced, greened, attractive, accessibility considered environment, whilst promoting sustainable modes of transport via its focus on pedestrians and cycling and its creation of a bespoke taxi facility.
- 11.2 The proposals would result in less than substantial harm to the Grade II Listed building (The Queens Hotel) by virtue of the extent of demolition of the station boundary and its proposed replacement with the new kiosk and lift tower building and its steps and the creation of a cycle hub in the underground mill goit and arches. The level of harm is considered to be tolerable when balanced against the public benefits of the scheme.
- 11.3 Therefore the proposal is in accordance with the Development Plan and is considered to be acceptable and is recommended for planning approval and listed building consent approval subject to the conditions set out in the Appendix.

### **Background Papers:**

PREAPP/19/00511

20/01996/LI

20/02048/FU



**Leeds**  
CITY COUNCIL

Originator: Sarah McMahon

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## **Appendix 3 – Updated Draft Conditions**

### **CITY PLANS PANEL**

**Subject: Planning Application 20/02048/FU for the demolition of existing structures, the construction of new cafe and cycle hub; a new taxi rank; public realm enhancement works including hard and soft landscaping; improvements to the cycle, pedestrian and multi-modal interchange arrangements and the installation of new access lift at Bishopgate Street, New Station Street And Mill Goit, Neville Street, And Dark Neville Street, Leeds City Centre.**

**And**

**Listed Building Application 20/01996/LI for the construction of a wall which will be built into an existing listed wall at Bishopgate Street, New Station Street, Leeds.**

### **20/02048/FU**

1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2) The development hereby permitted shall be carried out in accordance with the approved plans listed in the Plans Schedule.

For the avoidance of doubt and in the interests of proper planning.

3) No works shall begin on the development until a Statement of Demolition and Construction Practice has been submitted to and approved in writing by the Local Planning Authority. The Statement of Demolition and Construction Practice shall include full details of:

- a) details of any phasing of the development
- b) the means of access, location of site compound, storage and parking (including workforce parking), means of loading and unloading of all contractors' plant, equipment, materials and vehicles and associated traffic management measures
- c) the positions and types of cranes and details of excavations and earthworks
- d) the methods to be employed to prevent mud, grit and dirt being carried onto the public highway from the development hereby approved;
- e) measures to control the emissions of dust and dirt during construction;

- f) location of site compound, and its means of enclosure and of safety barriers including soundproofing barriers and vehicle crash barriers
- g) details of plant equipment/storage as well as details of any vibro-impact machinery;
- h) details and location of contractor and sub-contractor parking and of the management of abnormal loads
- i) a local resident and business communications strategy for the duration of the works
- j) how this Statement of Demolition and Construction Practice will be made publicly available by the developer
- k) the hours of construction activities

The approved details shall be implemented at the commencement of work on site, and shall thereafter be retained and employed until completion of works on site. Any changes to the approved details as the development progresses will be agreed in writing with the local planning authority prior to the commencement of construction of each subsequent phase. The Statement of Demolition and Construction Practice shall be made publicly available for the lifetime of the construction phase of the development in accordance with the approved method of publicity.

In the interests of highways safety and residential amenity in accordance with Saved Leeds Unitary Development Plan Review (2006) policy GP5 and the National Planning Policy Framework.

4) Development shall not commence until a Phase II Site Investigation Report has been submitted to, and approved in writing by, the Local Planning Authority. Where remediation measures are shown to be necessary in the Phase II Report and/or where soil or soil forming material is being imported to site, development shall not commence until a Remediation Statement demonstrating how the site will be made suitable for the intended use has been submitted to, and approved in writing by, the Local Planning Authority. The Remediation Statement shall include a programme for all works and for the provision of Verification Reports.

To ensure that the presence of contamination is identified, risks assessed and proposed remediation works are agreed in order to make the site 'suitable for use'.

5) If remediation is unable to proceed in accordance with the approved Remediation Statement, or where significant unexpected contamination is encountered, the Local Planning Authority shall be notified in writing immediately and operations on the affected part of the site shall cease. An amended or new Remediation Statement shall be submitted to, and approved in writing by, the Local Planning Authority prior to any further remediation works which shall thereafter be carried out in accordance with the revised approved Statement.

To ensure that any necessary remediation works are identified to make the site suitable for use.

6) Remediation works shall be carried out in accordance with the approved Remediation Statement. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site or phase of a site shall not be brought into use until such time

as all verification information has been approved in writing by the Local Planning Authority.

To ensure that the remediation works are fully implemented as agreed and the site has been demonstrated to be suitable for use.

7) Any soil or soil forming materials brought to site for use in garden areas, soft landscaping, public open space or for filling and level raising shall be tested for contamination and suitability for use. A methodology for testing these soils shall be submitted to, and approved in writing by, the Local Planning Authority prior to these materials being imported onto site. The methodology shall include information on the source of the materials, sampling frequency, testing schedules and criteria against which the analytical results will be assessed (as determined by risk assessment). Testing shall then be carried out in accordance with the approved methodology. Relevant evidence and verification information (for example, laboratory certificates) shall be submitted to, and approved in writing by, the Local Planning Authority prior to these materials being imported onto the site.

To ensure that contaminated soils are not imported to the site and that the development shall be suitable for use.

8) Prior to the commencement of demolition documentation demonstrating the absence or total removal of asbestos from any building(s) to be demolished shall be submitted to and approved in writing by the Local Planning Authority. Should documentation be unavailable or insufficient, post-demolition surface soil sampling of future landscaped or garden areas shall be carried out and the results shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of any post-demolition development.

Where surface soil sampling indicates remediation to be necessary, a Remediation Statement shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of construction. The Remediation Statement shall include a programme for all remediation works and for the provision of verification information.

Remediation works shall be carried out in accordance with the approved Remediation Statement. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site or that phase of the site shall not be brought into use until such time as all necessary verification information has been approved in writing by the Local Planning Authority.

To ensure that the site is safe and suitable for use

9) No demolition or development to take place until the applicant, or their agents or successors in title, has secured the implementation of a programme of archaeological recording. This recording must be carried out by an appropriately qualified and experienced archaeological consultant or organisation, in accordance with a written scheme of investigation which has been submitted by the applicant and approved in writing by the Local Planning Authority.

To ensure appropriate architectural and historic recording.

10) Prior to the commencement of construction details of existing and proposed ground levels, including external areas and the proposed finished floor levels relevant to the works within the development, shall be submitted to and approved in writing by the Local Planning Authority. Development shall be carried out in accordance with the approved details.

In the interests of amenity.

11) Prior to first use of the newly laid pedestrian areas a pedestrian wayfinding scheme, including alteration of existing signage shall be submitted to and agreed in writing with the Local Planning Authority. The approved signage scheme shall be implemented and completed before first use of the development

In the interests of highway safety.

12) The kiosk and cycle hub shall each not be occupied until provision has been made in the means of access both to and within the said kiosk and cycle hub, and within sanitary conveniences, for the needs of employees and members of the public who are disabled as well as non-gender specific provision. All such facilities shall thereafter be retained and made available for use.

In the interests of access for all.

13) The development hereby permitted shall not be occupied until full details of both hard and soft landscape works for that phase, including an implementation programme, have been submitted to and approved in writing by the Local Planning Authority. Hard landscape works shall include

- (a) proposed finished levels and/or contours,
- (b) boundaries, means of enclosure and retaining structures,
- (c) refuse and recycling bins store architectural screening;
- (d) other vehicle and pedestrian access and circulation areas,
- (e) hard surfacing areas,
- (f) minor artefacts and structures (e.g. artwork, furniture, play equipment, refuse or other storage units, signs, lighting etc.),

Soft landscape works shall include

- (h) planting plans including any additional opportunities for planting street trees
- (i) written specifications (including soil depths, cultivation and other operations associated with plant and grass establishment) and
- j) schedules of plants noting species, planting sizes and proposed numbers/densities including the trees proposed to address the requirements of Policy Land 2
- k) full details of and a management and maintenance plan for any green walls/screens including details of irrigation, feeding and the timescales and mean by which any failed planting will be removed and replaced
- l) A landscape management plan, including long term design objectives, management responsibilities and maintenance schedule

All hard and soft landscaping works shall be carried out and completed in accordance with the approved details, approved implementation programme and

British Standard BS 4428:1989 Code of Practice for General Landscape Operations. Confirmation in writing of the completion date shall be provided to the Local Planning Authority.

To ensure the provision and establishment of acceptable landscaping

14) a) No landscape work shall commence until full details of the load bearing cell type rooting zone using proprietary structures has been submitted and approved in writing by the local planning authority. Details shall be fully in accordance with LCC guidance on urban tree planting (available on Landscape Planning website).

Details shall include:

- Proprietary soil cell structures to support paving over extended sub-surface rooting areas
- Soil cell volume /soil volume calculations
- Specification of topsoil including additives and conditioners
- Tree grilles and guards and means of anchoring root balls. Built-in Root Irrigation Pipe system with end cap and aeration system.
- Passive and / or active irrigation including directed use of grey water / roofwater or surface water infiltration to benefit planted areas. Details of distribution system and controls
- Tree grill details
- Drainage system for tree pits.
- Where applicable -details of protection measures for statutory utilities and drainage
- Works shall then be carried out in accordance with the approved details.

b) To ensure full compliance, a brief report on the installation of the rooting zone structures, including supporting photographic evidence, shall be submitted to the LPA when the works are still “open” to allow LPA inspection prior to any surfacing works. Seven days written notice shall be given to the Local Planning Authority that the rooting zone structures are in place to allow inspection and approval of them as implemented on site.

NOTE-this item cannot be discharged until post inspection approval is confirmed.

c) A 3 year irrigation programme for the trees (in accordance with BS 8545-2014 Trees from Nursery to Independence) shall be submitted to the LPA for approval in writing. Confirmation of irrigation compliance shall be submitted to the LPA on a quarterly basis for the full 3 year programme period

To ensure the provision of amenity afforded by appropriate landscape design and its cultural requirements are integrated into the development scheme

15) If within a period of five years from the date of the planting of any tree/hedge/shrub that tree/hedge/shrub, or any replacement, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the Local Planning Authority, seriously damaged or defective, another tree/hedge/shrub of the same species and size as that originally planted shall be planted in the same location as soon as reasonably possible and no later than the first available planting season or such other timescale as is agreed in writing by the Local Planning Authority.

To ensure maintenance of a healthy landscape.

16) No external lighting shall be installed unless a scheme has been approved in writing by the Local Planning Authority. The scheme shall be implemented in accordance with the approved details. The source of external lights should be designed to not materially impact nearby sensitive receptors which shall be defined as residential dwellings and any highways infrastructure such as traffic lights.

In the interests of amenity.

17) No building works of the new kiosk building, lift tower, external steps, the taxi shelter and cycle hub entrance until details and samples of all external walling and roofing materials have been submitted to and approved in writing by the Local Planning Authority. Samples shall be made available on site prior to the commencement of building works, for inspection by the Local Planning Authority which shall be notified in writing of their availability. The building works shall be constructed from the materials thereby approved.

In the interests of visual amenity.

18) No construction of the demountable perforated panels to the walls and soffit under the Neville Street Bridge and of the temporary architectural frontages to the arches along Dark Neville Street shall commence until details and samples of all external walling and roofing materials have been submitted to and approved in writing by the Local Planning Authority. Samples shall be made available on site prior to the commencement of building works, for inspection by the Local Planning Authority which shall be notified in writing of their availability. The building works shall be constructed from the materials thereby approved.

In the interests of visual amenity.

19) No building works of the new kiosk building, lift tower, the cycle hub entrance and the taxi shelter until full details of the following have been submitted to and approved in writing by the Local Planning Authority-

(i) 1 to 20 scale drawings of the detail of roof line treatments and ground floor treatments

(ii) 1 to 20 scale section drawings of window details and the depths of any reveals

(iii) 1 to 20 scale section drawings of all new external doors and entrances

(iv) 1 to 20 scale section drawings of the junctions of different materials.

The works shall be carried out in accordance with the details thereby approved.

In the interests of visual amenity.

20) No permanent surfacing works shall take place to the hard surfaced paved and trafficked areas and street and the landscaped areas until details and samples of all surfacing materials to the said hard surfaced and landscaped area have been submitted to and approved in writing by the Local Planning Authority. The surfacing works shall be constructed from the approved materials.

In the interests of visual amenity.

21) The new kiosk building shall not be occupied until, where relevant, details for the provision of bin stores (including siting, materials and means of enclosure) and (where applicable) storage of wastes and access for their collection shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be implemented in full before the occupation of the phase.

In the interests of amenity.

22) Details of any external extract ventilation system for commercial food premises shall be submitted to and approved in writing by the Local Planning Authority prior to its installation. The system shall be implemented in accordance with the approved details.

Hot food uses will often require an extract ventilation system to deal with odour and fumes. Guidance on suitable design is provided in DEFRA guidance at: <http://www.defra.gov.uk/publications/files/pb10527-kitchen->

In the interests of amenity.

23) Details of the highway design of the scheme on Neville Street, Swinegate, Mill Hill, Bishopgate Street and Dark Neville Street shall be submitted and agreed in writing by the Local Planning Authority prior to commencement of construction of the highway works, details shall include:

- a) The means of control and management of the taxi rank and the feeder rank to it.
- b) Layout of loading bays.
- c) Layout and operation of pedestrian crossing points
- d) Operation of the traffic signals including intelligent technology
- e) The cycle priority scheme on Dark Neville Street including the junction with Neville Street
- f) Changes to traffic signs and lining.

The works shall be implemented in accordance with the approved details.

To ensure the free and safe use of the highway during all development works and throughout the lifetime of the development.

24) The taxi operational elements of the development shall not be brought in to use until a Taxi Operations Management Plan, including details of how taxis will be called from the feeder ranks to the new taxi shelter ranks and how the ranks and taxi waiting shelter will be maintained, has been submitted to and approved in writing by the Local Planning Authority. The plan shall be fully implemented and operated in accordance with the approved timescales.

To ensure the free and safe use of the highway in accordance with adopted Leeds Core Strategy policy T2.

25) Before development commences, the detailed SuDS based Drainage Scheme based on the principles of The SUDS Manual (C753) with design criteria as set out within the Council's Minimum Development Control Standards for Flood Risk should be submitted and approved in writing by the Local Planning Authority. The

application should provide suitable drainage drawings, summary calculations and results of all investigations detailing the surface water drainage works as set out below. The final drainage design shall be in line with the drainage strategy as set out within the Statement of Intent – Drainage Strategy prepared by Atkins (Ref 5193758 dated 20/06/2020) unless otherwise agreed with the LPA. The works shall be implemented in accordance with the approved scheme before the development is brought into use, or as set out in the approved phasing details.

The detailed design drawings, calculations and supporting information shall include the following:

- (i) Model Information (Micro Drainage or similar approved) to include a plan showing pipework model numbering and network details,
- (ii) Results: Summary of Results showing all the modelling criteria and summary network results for critical 1 in 2 year, 1 in 30 year and 1 in 100 year plus 40% CC storm events showing maximum water level, flow and velocity and details of any surface flooding anticipated.
- (iii) A drawing showing the proposed impermeable areas, suitably annotated.
- (iv) Calculations and any supporting survey and investigations to justify and demonstrate the existing and proposed discharge rate.
- (v) Drainage Plan showing drainage layout, manholes including cover and invert levels, proposed levels, pipe sizes and gradients, all on -line controls, on and off line storage structures and outfall details.
- (vi) Plan showing overland exceedance routes in the event of a failure of the drainage system or storm event in excess of the 1 in 100 + 40% CC storm event.
- (vii) Summary Drainage Report setting out the Drainage Strategy and results of the calculations demonstrating compliance with the above.
- (viii) Where third party agreements to construct sewers and to discharge flows are required, then written evidence of these two agreements shall be provided.
- (ix) Timetable for implementation of the drainage works including an assessment of any phasing of the development.
- (x) Demonstrating that adequate water quality of the off- site surface water flows in accordance with the Simplified Index Approach as set out within Section 26 of the SUDS Manual (C753) can be achieved during all phases of the development.
- (xi) Where SUDs are only proposed in part or not at all, then a full justification statement shall be provided to demonstrate why it is not considered appropriate or reasonable.

To ensure sustainable drainage and flood prevention in accordance with Adopted Natural Resources and Waste Local Plan Policy Water 7 and Policy GP5 of the Leeds Unitary Development Plan Review 2006.

26) Development shall not commence until details and a method statement for interim and temporary drainage measures during the demolition and construction phases have been submitted to and approved in writing by the Local Planning Authority. This information shall provide full details of who will be responsible for maintaining such temporary systems and demonstrate how the site will be drained to ensure there is no increase in the off-site flows, nor any pollution, debris and sediment to any receiving watercourse or sewer system. Where temporary discharges to a sewer are proposed, written confirmation from the sewer owner that these have been accepted shall be provided. The site works and construction phase shall thereafter be carried out in accordance with approved method statement,

unless alternative measures have been subsequently approved by the Planning Authority

To prevent flooding offsite in accordance with the National Planning Policy Framework.

27) ) Prior to the occupation of either the kiosk or cycle hub, whichever is occupied first, details shall be provided in respect to the management, inspection and maintenance of any non-adopted drainage features. The details shall identify the responsible parties and set out how these will be funded and managed and provide a schedule of the proposed inspections and annual maintenance for the lifetime of the development. The plan shall be submitted to and approved in writing with the Local Planning Authority prior to first occupation and the development shall thereafter be maintained at all times in accordance with the approved details.

To ensure the development is adequately maintained for the lifetime of the development.

28) No construction works in the relevant area (s) of the site shall commence until measures to protect the public 450 mm water main that is laid within the site boundary have been implemented in full accordance with details that have been submitted to and approved by the Local Planning Authority in consultation with the relevant statutory undertaker. The details shall include but not be exclusive to the means of ensuring that access to the pipes for the purposes of repair and maintenance by the statutory undertaker shall be retained at all times. If the required stand -off or protection measures are to be achieved via diversion or closure of the infrastructure, the developer shall submit evidence to the Local Planning Authority that the diversion /closure has been agreed with the relevant statutory undertaker and that , prior to construction in the affected area (s), the approved works have been undertaken.

In the interest of public health and maintaining the public sewerage and water supply network.

### **20/01996/LI**

1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2) The development hereby permitted shall be carried out in accordance with the approved plans listed in the Plans Schedule.

For the avoidance of doubt and in the interests of proper planning.

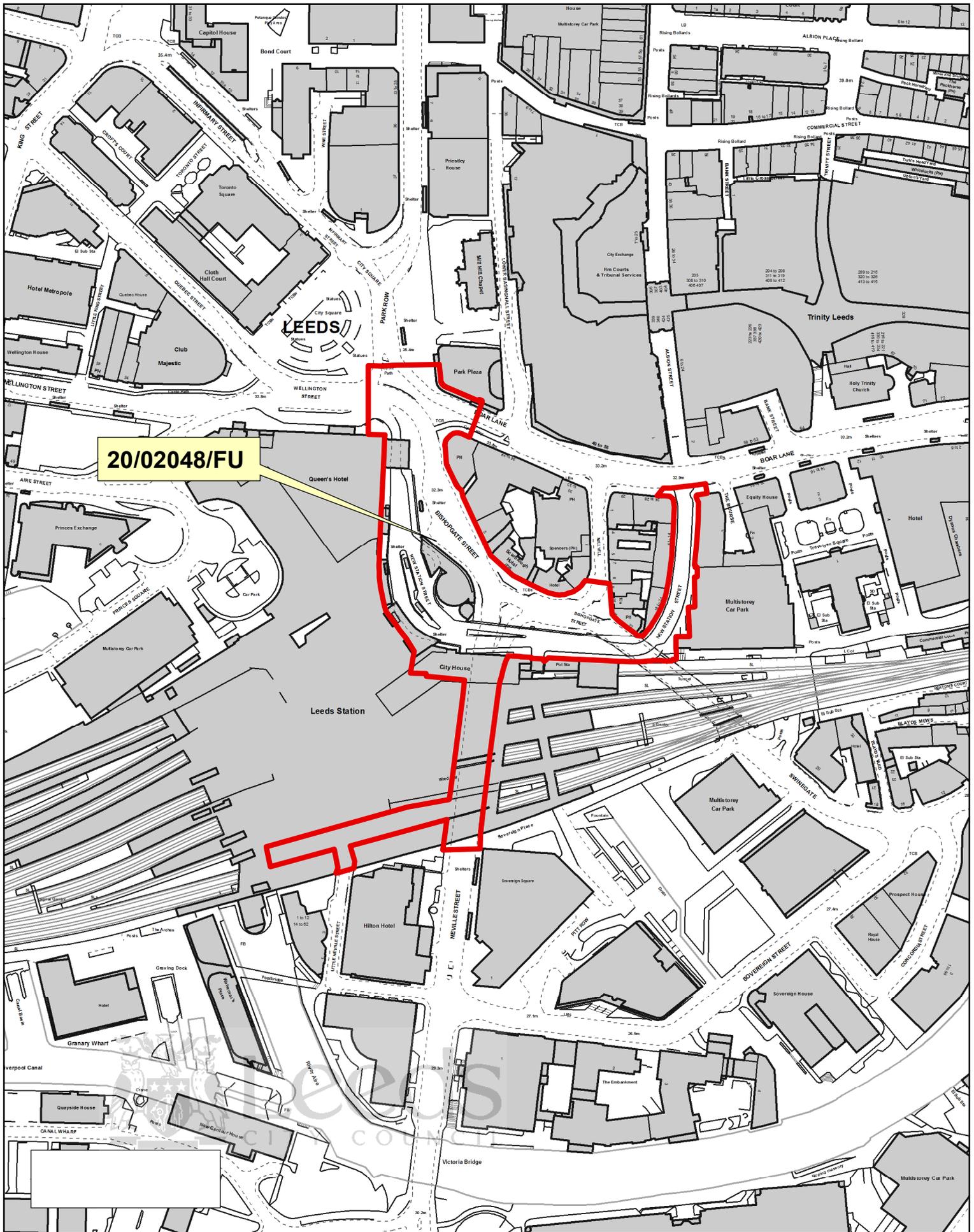
3) No building works of the new external steps and wall shall commence until details and samples of all external materials have been submitted to and approved in writing

by the Local Planning Authority. Samples shall be made available on site prior to the commencement of building works, for inspection by the Local Planning Authority which shall be notified in writing of their availability. The building works shall be constructed from the materials thereby approved.

In the interests of visual amenity.







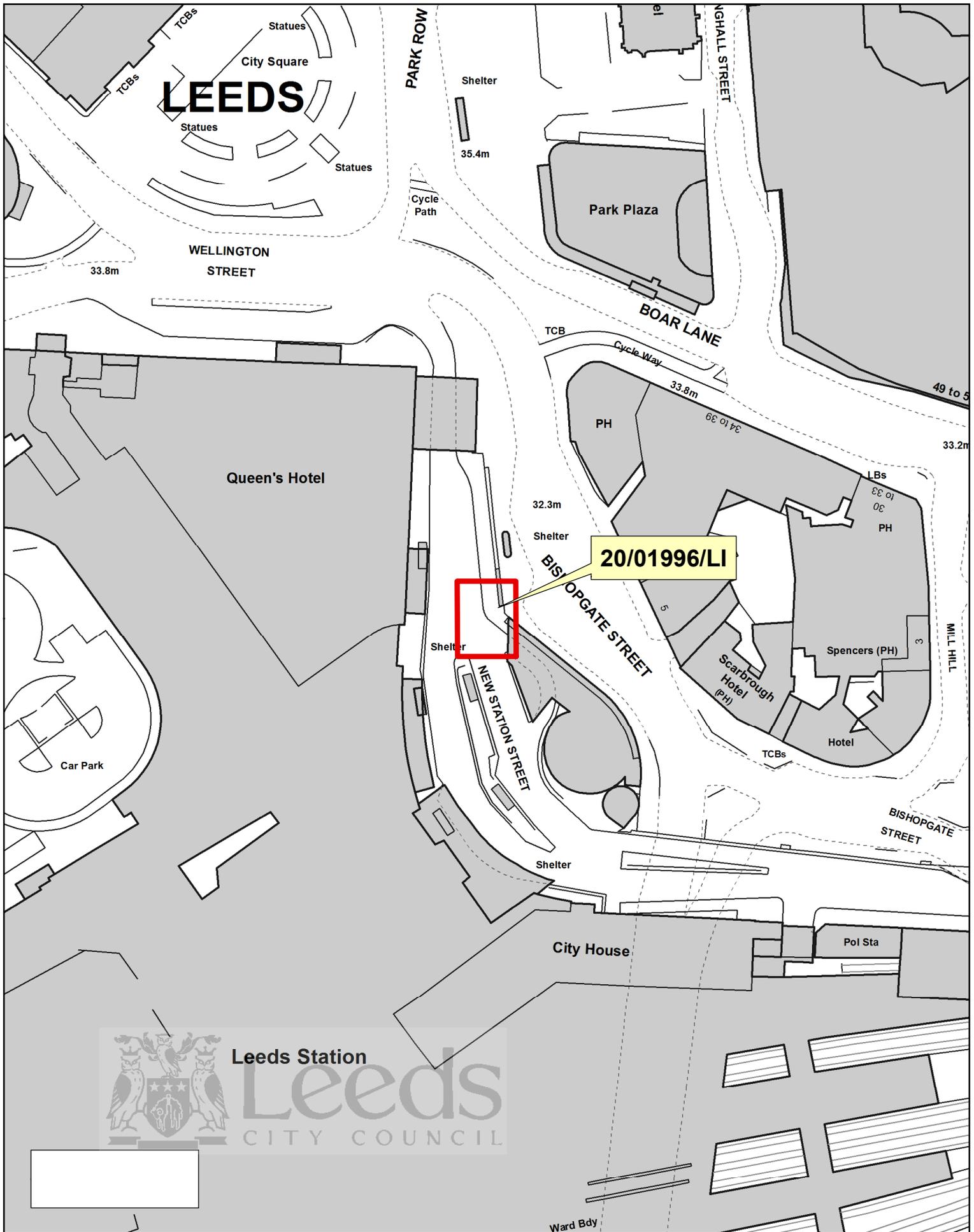
# CITY PLANS PANEL

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SCALE : 1/2500





# CITY PLANS PANEL

